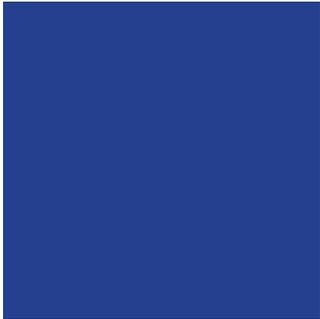




Main Street Corridor Streetscape Master Plan



Adopted February 14, 2008

CITY OF FOUNTAINS
HEART OF THE NATION



KANSAS CITY
MISSOURI

RESOLUTION NO. 080074

Adopting the Main Street Corridor Streetscape Master Plan for that area that extends one-half block to the east and west of Main Street from 30th Street to 44th Street.

WHEREAS, the City Development Department has conducted a study delineating the characteristics of the Main Street Streetscape study area; and

WHEREAS, the City Development Department, partnered with the Main Street Corridor Development Corporation, a financial contributor to the plan, to conduct a public planning process involving the community; and

WHEREAS, as a result of said study, the City Development Department has proposed a framework for future streetscape improvements within the study area; and

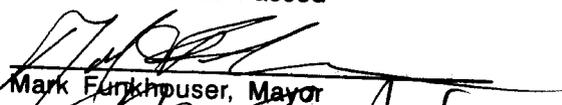
WHEREAS, the City Development Department has incorporated its findings in the Main Street Streetscape Plan; NOW, THEREFORE,

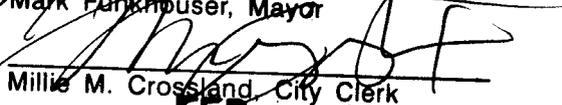
BE IT ORDAINED BY THE COUNCIL OF KANSAS CITY:

Section 1. That the Main Street Corridor Streetscape Master Plan, which will serve as a comprehensive guide for future streetscape improvements within the planning area that extends one-half block to the east and west of Main Street from 30th Street to 44th Street, is hereby adopted in a form attached hereto and incorporated herein.

Authenticated as Passed




Mark Furkhouser, Mayor


Millie M. Crossland, City Clerk

Date Passed

FEB 14 2008

ACKNOWLEDGEMENTS

City Council

The Honorable Deb Hermann
First District-at-Large

The Honorable Bill Skaggs
First District

The Honorable Ed Ford
Second District-at-Large

The Honorable Russ Johnson
Second District

The Honorable Melba Curls
Third District-at-Large

The Honorable Sharon Sanders Brooks
Third District

The Honorable Beth Gottstein
Fourth District-at-Large

The Honorable Jan Marcason
Fourth District

The Honorable Cindy Circo
Fifth District-at-Large

The Honorable Terry Riley
Fifth District

The Honorable Cathy Jolly
Sixth District-at-Large

The Honorable John Sharp
Sixth District

Mayor

The Honorable Mark Funkhouser

City Manager

Wayne A. Cauthen

Board of Parks and Recreation Commissioners

Mr. John Fierro - President

Mr. Tyrone Aiken - Commissioner

Ms. Frances Semler - Commissioner

Ms. Aggie Stackhaus - Commissioner

Mr. Ajamu K. Webster - Commissioner

ACKNOWLEDGEMENTS

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Ms. Anna Graether
Mr. Frank Haar
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Mr. Rick Jones
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INTRODUCTION



PLAN BACKGROUND

The **Main Street Corridor Land Use and Development Plan**, adopted by the City of Kansas City, Missouri by Resolution 021500 on January 9, 2003, provides the corridor with policy direction related to land use, urban design, priority development / redevelopment areas and implementation tools. A key implementation measure of the Main Street Corridor Land Use and Development Plan is to create a streetscape plan for the area. In spring 2007, the Main Street Corridor Development Corporation (MainCor) partnered with the City of Kansas City, Missouri to undertake the Main Street Corridor Streetscape Master Plan.



PLANNING PROCESS

The **Main Street Corridor Streetscape Master Plan** was designed to be a public planning process involving area residents, businesses and institutions. The decision making body was made up of a citizen steering committee, appointed by the Mayor, that represented different segments of the corridor and adjoining neighborhoods. The consultant team and members of City Staff provided technical support in this process.

PLAN PURPOSE

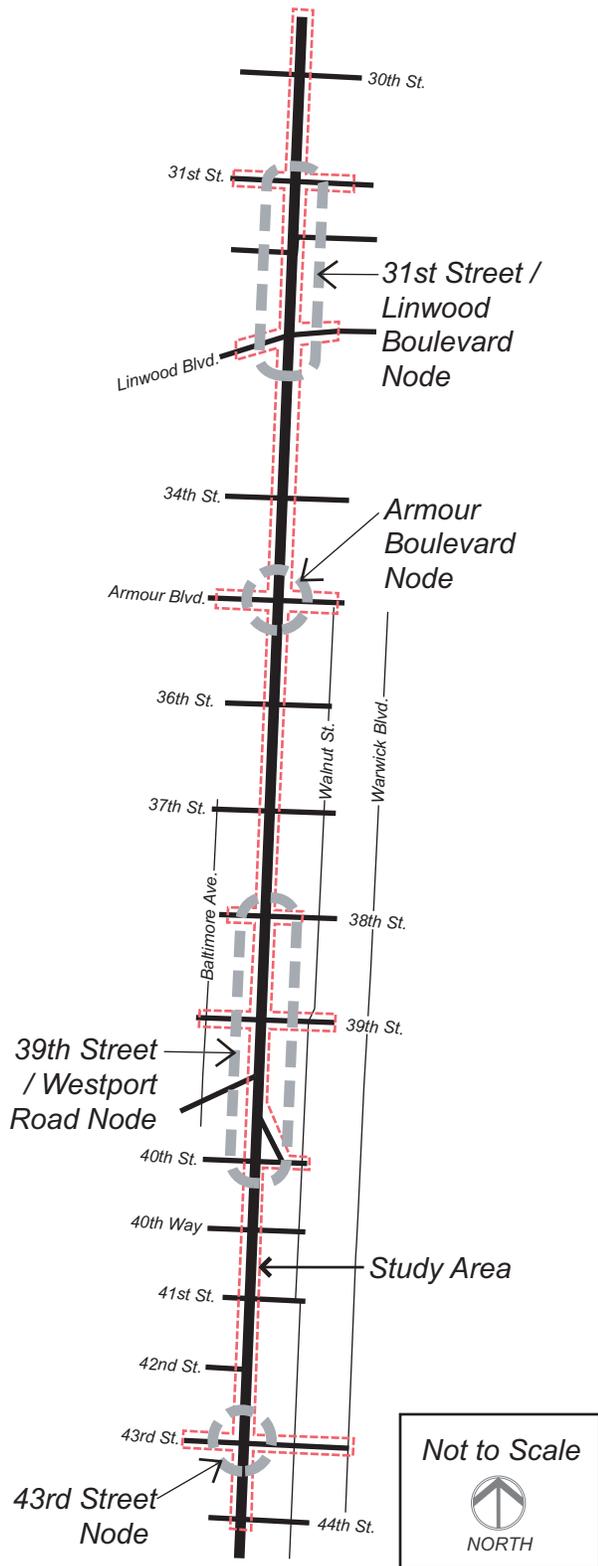
The Main Street Corridor Streetscape Master Plan will:

- Serve as a comprehensive guide for future streetscape improvements along the corridor.
- Focus on appropriate and necessary streetscape treatments to the public right of way.
- Provide general recommendations for elements outside of this area.



Any recommendations made in this document will be further refined during the design and construction phase of streetscape development. The purpose of this document is to achieve a unified, cohesive streetscape framework that will accent the surrounding business and neighborhoods.

INTRODUCTION



PROJECT STUDY AREA

The Main Street Corridor Streetscape Master Plan study area has been defined by the City of Kansas City and MainCor as: Main Street from 30th Street to 44th Street with expansion east and west one block. (see fig. 1.1)

Along the lines of the Main Street Corridor Land Use and Development Plan, this Plan is organized around the following four nodes within the corridor. (see fig. 1.1)

- 31st Street / Linwood Boulevard Node
- Armour Boulevard Node
- 39th Street / Westport Road Node
- 43rd Street Node

PLAN ORGANIZATION

The Main Street Corridor Streetscape Master Plan is divided into four (4) sections:

- **Section 1: Existing Conditions**
 - general existing corridor infrastructure and conditions and its impact on the streetscape plan
- **Section 2: Goals and Recommendations**
 - general goals and recommendations
- **Section 3: Concept Plans**
 - block by block examination with specific recommendations
- **Section 4: Implementation**
 - phasing of construction and cost opinions

fig. 1.1 - project study area

SECTION 1: EXISTING CONDITIONS

EXISTING CONDITIONS

Prior to initiating the planning process, an existing conditions survey was completed to provide a “snapshot” of the project area. The purpose was to provide a basis for improvement and understanding of how the area is used today. While the project scope is primarily focused on “curb to property line” improvements, the entire planning area was inventoried in order to identify issues related to possible streetscape improvements.

The following items were evaluated:

- Street Pavement
- Storm Utilities
- Curbs
- Sidewalks
- Parking
- Street Signage
- Traffic Signals
- Street Lights
- Pedestrian Lights



fig. 1.2 - mnookin plaza



fig. 1.3 - street in need of repair



fig. 1.4 - clogged and old catch basin

Street Pavement

The condition of the street pavement appears to be adequate. However, potholes are present and in need of treatment and some repairs are not consistent. Inconsistencies in the street pavement give users the impression of an unfinished streetscape and create the potential for hazardous situations. (see fig. 1.3)

Storm Utilities

All sewer main lines within the study area are combined storm and sanitary. This poses problems in large rain events.

There are many catch basins that need to be replaced, cleaned or repaired. Although there is not a need for total replacement, a survey would aid in evaluating the current condition and loads on the system. Such a survey may identify the need for additional catch basins. (see fig. 1.4)

Many catch basins need to be moved in order to comply with the Americans with Disabilities Act Accessibility Guidelines (ADAAG) at crosswalk intersections.

SECTION 1: EXISTING CONDITIONS

EXISTING CONDITIONS

Curbs

Approximately 50% of the curbs within the corridor are deteriorated, inadequate or vary in appearance. Lack of maintenance, snow plow damage and general age has contributed to this condition.

For curbs to be a unified and functional piece of infrastructure, a standard style should be established throughout the corridor.

As curb replacement occurs, it should be coordinated with improvements to street drainage and sidewalks.

(see fig. 1.5)



fig. 1.5 - dilapidated curb

Sidewalks

About 50% of the sidewalks within the corridor are in poor condition. Although there is not a need for total sidewalk replacement, targeted sidewalk restoration is necessary.

As sidewalks are repaired and improved, it is recommended that they are evaluated on a block by block basis. Similar to the curbs, the sidewalk style should be consistent to create a uniform appearance throughout the corridor. Right of way / property lines should be verified in order to maximize the width of sidewalks. Other infrastructure needs that should be evaluated at the time of replacement include: curbs, crosswalks, storm inlets, utilities, street signs, traffic signals, and street lights.

(see fig. 1.6 - 1.8)

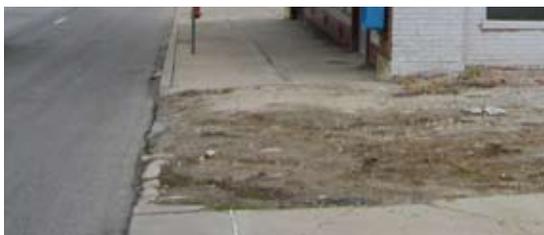


fig. 1.6 - missing sidewalk



fig. 1.7 - narrow sidewalk



fig. 1.8 - narrow sidewalk

SECTION 1: EXISTING CONDITIONS

EXISTING CONDITIONS

Parking

Although the need for additional parking is an issue, many businesses have adjacent private parking. The only prohibition for “on-street” parking on Main Street is associated with the KCATA MAX bus line. The northbound outside lane is “bus only” between 7-9 am and the outer southbound lane is between 4-6 pm. (see fig. 1.9)



fig. 1.9 - existing street parking

Street Signage

There are a variety of public signs that line the edge of the street. Although many are necessary, some are redundant, which makes them hard to understand and creates visual clutter. The signage pole style is inconsistent throughout the corridor; most are galvanized square stock but some are round tubes. In addition, there are many places where a sign has been removed but the pole or a portion of the pole remains.

In order to provide a consistent, easily readable and aesthetic signage package, a standard should be identified throughout the corridor. Also, all redundant signage should be consolidated in order to reduce the number of signs. (see fig. 1.10)



fig. 1.10 - existing signage clutter

SECTION 1: EXISTING CONDITIONS

EXISTING CONDITIONS

Traffic Signals



fig. 1.11 - existing traffic light at armour boulevard

Traffic signals and pedestrian crosswalk lights have been updated with the exception of the Armour Boulevard intersection, which is scheduled to be replaced. All existing traffic lights appear to be fully functional and are operating correctly. With the installation of any new traffic signal, incorporate pedestrian countdown, audible cue indicators, and all other regulations established by the Americans With Disabilities Act. (see fig. 1.11)

Street Lights



fig. 1.12 - existing street light

From 34th Street north to 31st Street there is ample light at the ground surface. Most of the cobra head street lights are functioning and in good condition. Generally, south of 34th Street there are street lights on one side of the road. Although meeting city roadway standards, one sided lighting creates inconsistent lighting conditions making the unlit side of the roadway seem darker. There seems to be a correlation between the unlit side of the roadway and the presence of vacant buildings and criminal activity. (see fig. 1.12)

Pedestrian Lights

Few locations in the study area have pedestrian scale lighting, such as at 31st Street and Main Street. The design team was notified of existing conduit located on the east side of Main Street between 39th Street and 40th Street that is available for pedestrian lights. Special consideration should be taken to accent key areas or nodes with pedestrian lights.

SECTION 2: GOALS AND RECOMMENDATIONS



GOALS

The following are a series of general goals that are meant to implement the policies of the Main Street Corridor Land Use and Development Plan, improve the corridor aesthetics and address concerns of the Steering Committee and the community. *(The goals are further defined by a series of recommendations discussed in this section.)*

- Improve the multi-modal environment of the corridor.
- Provide a clear transition between commercial areas and residential neighborhoods while still providing strong connections for pedestrians and motorists.
- Improve ease of orientation and “sense of place” throughout the corridor.
- Reinforce north/south and east/west connections within corridor.
- Increase the sense of safety and visual aesthetics of Main Street.
- Provide consistency in design elements to help unify the corridor, while maintaining the unique strengths of each activity node.
- Provide a safe and pleasant separation of pedestrians and vehicles.
- Utilize public investment in infrastructure and aesthetics to encourage private reinvestment in the corridor.

SECTION 2: GOALS AND RECOMMENDATIONS

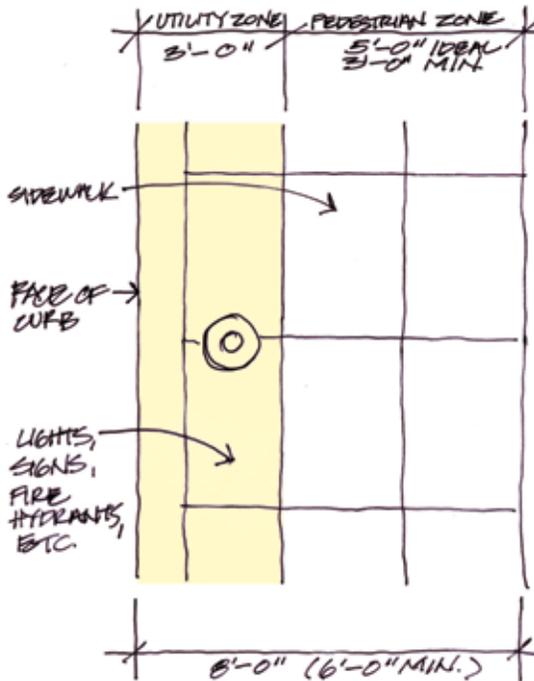


fig. 2.1 - plan view - typical sidewalk

Recommendation 1:

Enhance Sidewalk Conditions

Improving the pedestrian environment of the corridor is an important objective of this plan. Although the corridor is developed and there is no discussion of moving the existing curb line, there are areas in which sidewalks could be widened into existing public right of way. In these areas it is recommended that whenever possible:

- Sidewalk widths be increased to a minimum of 7'-8' wide.
- Establish a minimum of 3' from the face of curb line for the placement of utilities, lighting and signage.

(see fig. 2.1 and 2.2)

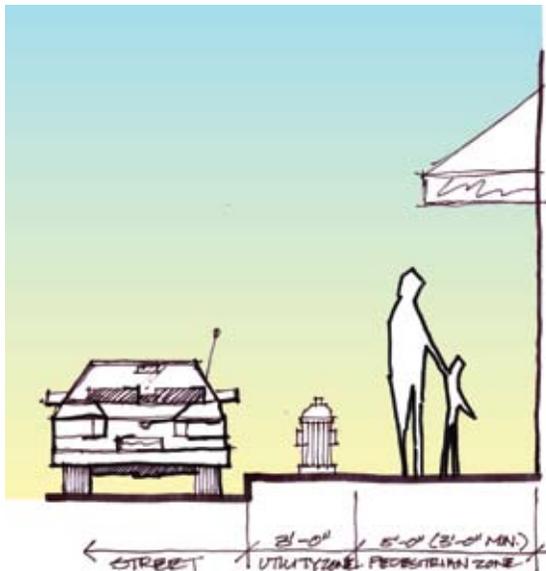


fig. 2.2 - elevation - typical sidewalk

SECTION 2: GOALS AND RECOMMENDATIONS

Recommendation 2:

Update / Repair Infrastructure

Infrastructure should be updated or repaired to the city's standard. Investment in the area's infrastructure and improvement of aesthetics will encourage private investment.

Key Infrastructure Items include:

- Sidewalk replacement and repair.
- Curb replacement and repair.
- Selected crosswalk definition (align curb ramps, stripe crosswalk). *(see Section 3 for specific areas)*
- Catch basin repair, upgrade and cleaning.
- Traffic signal installation at the Armour Boulevard and Main Street intersection per the Parks & Recreation Department Standards.



fig. 2.3 - example of striped crosswalk

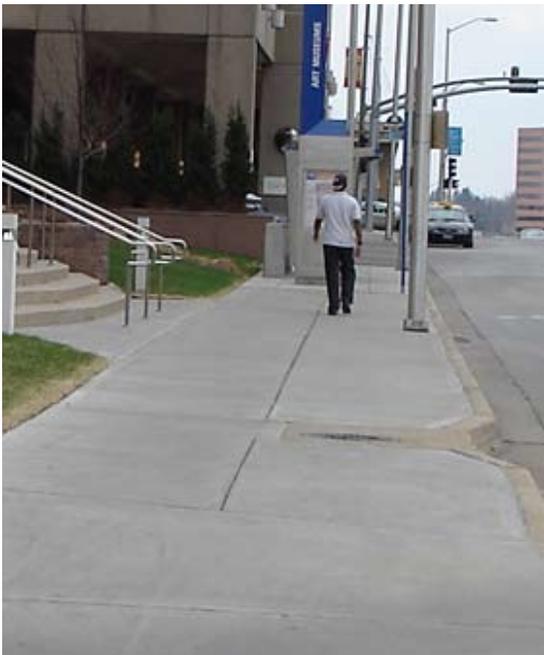


fig. 2.4 - example of upgraded catch basin and sidewalk.

SECTION 2: GOALS AND RECOMMENDATIONS

Recommendation 3:

Provide Uniform Lighting (Roadway and Sidewalk)

The northern portion of the corridor has adequate lighting; south of 34th Street there are street lights on one side of the roadway. Although City street lighting standards are met, the absence of street lights on one side of the road makes the corridor seem dark and corresponds with the area that has highest crime levels. In order to improve this condition, incorporate street lights where they are lacking to ensure light level uniformity (see fig. 2.5)

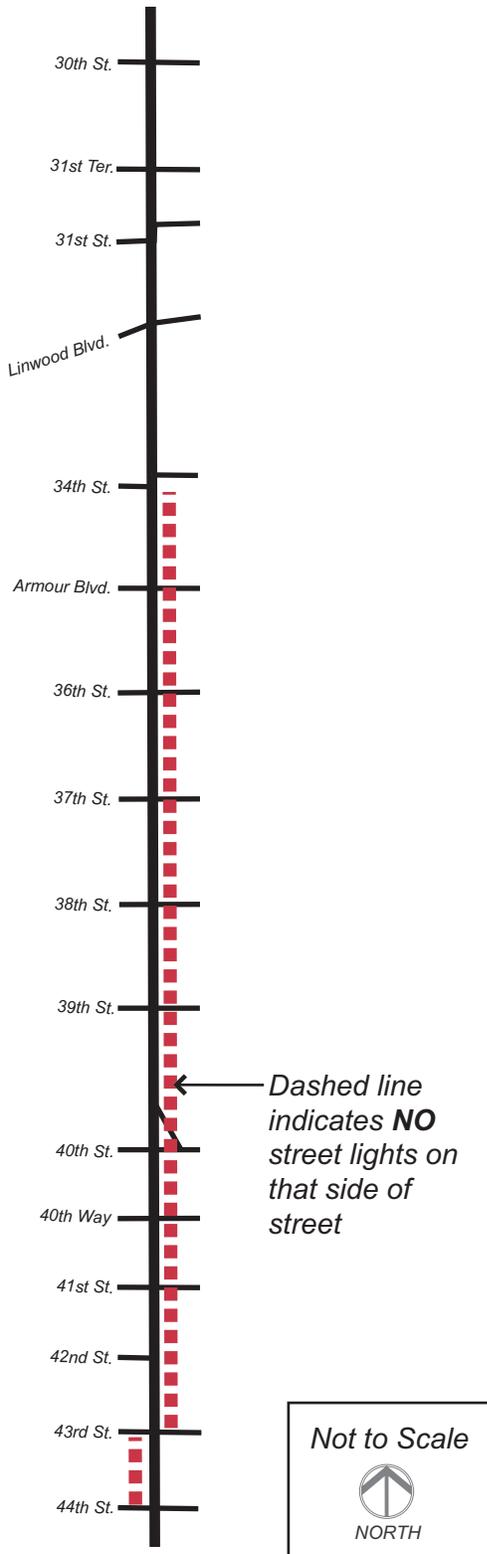


fig. 2.5 - street light diagram

SECTION 2: GOALS AND RECOMMENDATIONS

Recommendation 4:

Consolidate Regulatory Signage (No Parking, Bus Signage etc.)

Pursue signage consolidation to reduce visual clutter and improve appearance. As part of this effort, it is recommended that:

- All signage poles become uniform in style, color and height.
- Redundant signage be removed or consolidated.



fig. 2.6 - redundant signage

SECTION 2: GOALS AND RECOMMENDATIONS

Recommendation 5:

Define Street Edge with Streetscape

Defining the edge of the streetscape (from edge of curb to property line) will help to create perception of narrowing the roadway. This perception will help slow vehicular traffic and improve the pedestrian environment. This improvement can be achieved by incorporating the appropriate edge treatments.

Potential Edge Treatments:

- Plantings
- Screens and fences
- Low walls
- Incorporation of art
- Roadway and sidewalk lighting
- Building infill

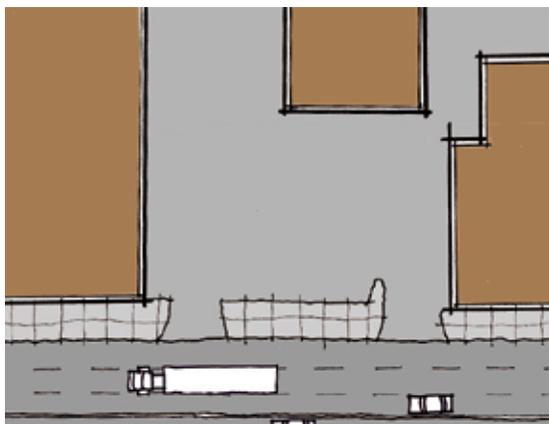


fig. 2.7 inconsistent building setbacks weaken edge of street

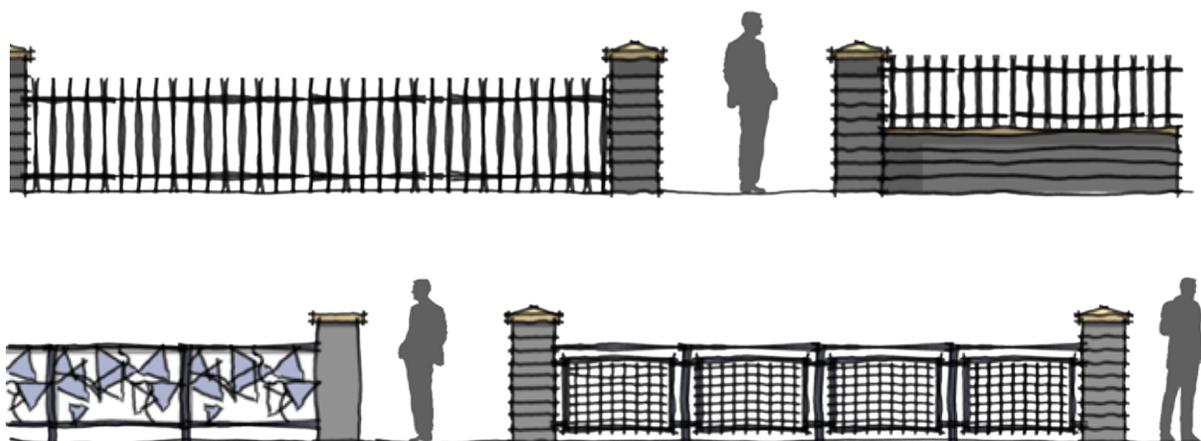


fig. 2.8 - example of edge treatment

SECTION 2: GOALS AND RECOMMENDATIONS

Recommendation 6:

Incorporate Markers and Streetscape Treatments

Markers and streetscape treatments will help to identify neighborhoods and provide separation between Main Street commercial areas and adjoining residential areas.

- Install neighborhood markers approximately one half block east or west of Main Street on side streets that enter residential neighborhoods.
- Scale neighborhood markers to the pedestrian, but also ensure markers are recognizable to vehicular traffic.
- Develop markers that are unique to each area but with some commonality to tie them together throughout the corridor. (see fig 2.9)
- Include neighborhood associations in the design and placement of the markers.

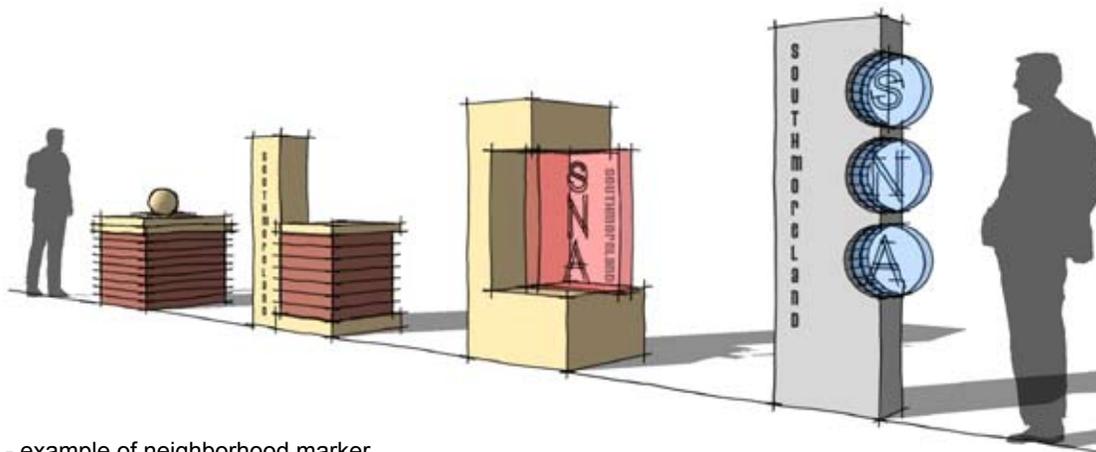
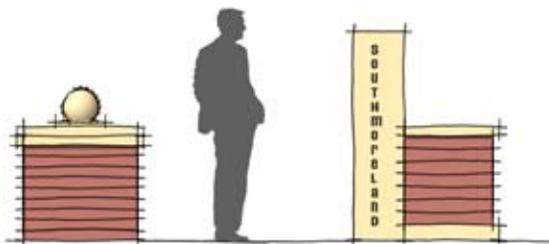
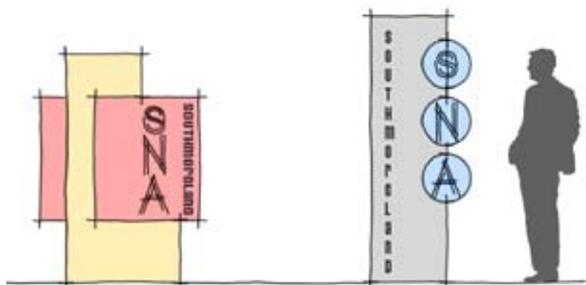


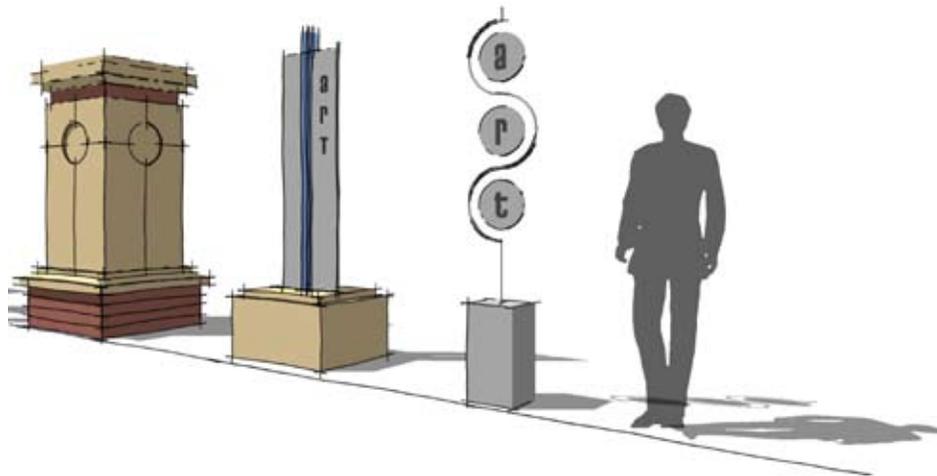
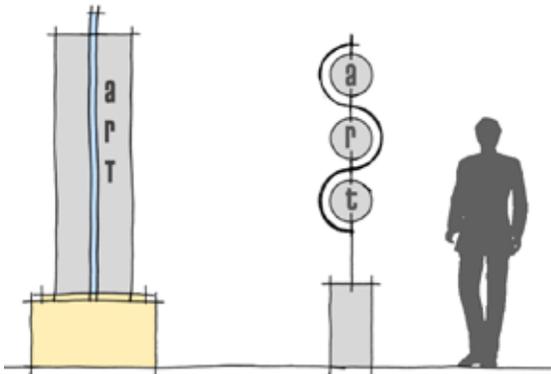
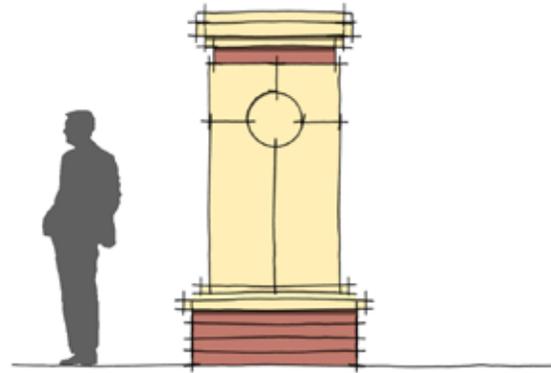
fig. 2.9 - example of neighborhood marker

SECTION 2: GOALS AND RECOMMENDATIONS

Recommendation 7:

Incorporate Unique Gateway Markers and Streetscape Treatments

Implement gateway markers at key nodes / subdistricts to help notify the pedestrian and driver that they are entering the Main Street Corridor. This will reinforce the north/south connections, add to the corridor's sense of place and improve the ease of orientation.



- Gateway markers should be larger than pedestrian scale and easily identifiable.
- Gateway markers should be carefully placed in order to achieve the best visibility for all users.
- Gateway markers may also act as informational kiosks with maps orienting and informing the pedestrians about the corridor.
- Each mixed-use node should have a gateway marker specific to that node. (see fig. 2.10)

fig. 2.10 - example of gateway markers

SECTION 2: GOALS AND RECOMMENDATIONS

Recommendation 8:

Transform the Exterior of Utility Elements Into Visual Assets

Currently, utility equipment within the corridor is a popular target for graffiti and vandalism. The idea of beautifying the equipment and providing an unexpected corridor unifying element, without hindering functionality, was identified as a priority by the Steering Committee and the public in the planning process. The specifics of this program shall be defined for the corridor in the design / construction of the initial streetscape project.

Potential visual asset utility elements:

- Fire hydrants (example- fire hydrants with Community Improvement District colors and logo similar to Columbus Park.) (see fig. 2.12) Coordination with KCMO Water and KCFD will occur prior to implementation.
- Gas meters (painted a consistent color.)
- Traffic signal boxes (interpretive signage or artwork.) (see fig 2.13)



fig. 2.11 - examples of historic images which could be applied to the switch gear boxes



fig. 2.12 - columbus park fire hydrants



fig. 2.13 - examples of what the traffic signal boxes could look like with applied graphics

SECTION 2: GOALS AND RECOMMENDATIONS

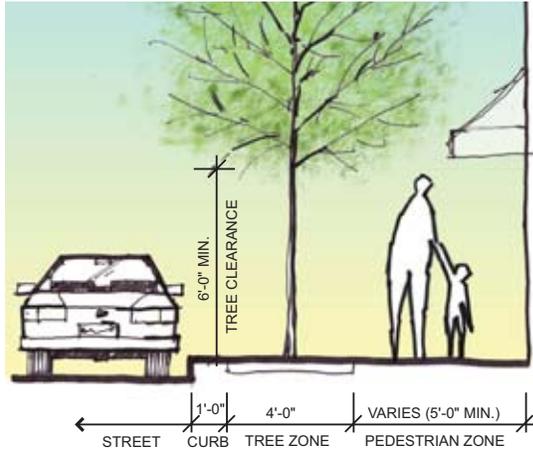


fig. 2.14 - elevation of typical sidewalk with street tree and tree grate

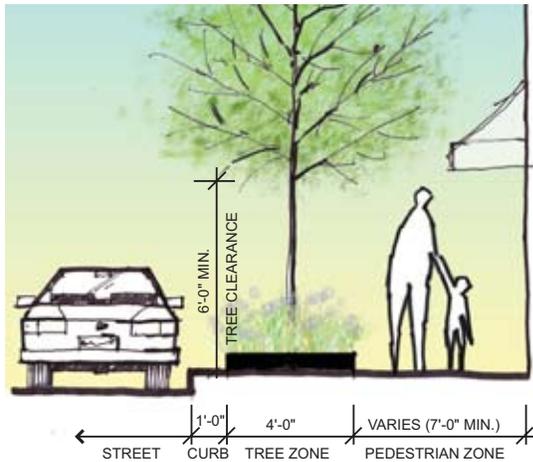


fig. 2.15 - elevation of typical sidewalk with street tree in planter



fig. 2.16 - infiltration planter at 12th street

Recommendation 9:

Incorporate Street Trees and Landscaping in Key Areas

Plant street trees within appropriately sized sidewalks. The benefits of street trees include:

- Enhancement of an edge between the sidewalk and curb line.
- Strengthening the north-south rhythm of the street experience.
- Shade for pedestrians.
- Greening of the corridor.

As implementation begins MainCor will work with the City to insure street tree installation will be appropriate and successful. Special consideration should be made for utilities, vaults in sidewalks and visibility of adjacent businesses. Additional plantings are encouraged on private property where sidewalk width does not allow. When appropriate and agreed to by the private property owner, tree planters may include both public and private land.

(Section 3 identifies where street trees are recommended and Appendix B identifies specific trees per block.)

Sidewalk Considerations for Street Tree Application:

- Minimum of 10' sidewalk width, utilize tree grates for additional walking surface. *(see fig. 2.14)*
- Minimum of 12' sidewalk width, utilize open planters for color. *(see fig. 2.15)*

Best Management Practices (BMPs)

Alternative storm water solutions should be considered in the design / construction phase, examples include: storm water inlet alternatives, rain gardens and drought tolerant plants. *(see fig. 2.16)*

SECTION 2: GOALS AND RECOMMENDATIONS

Recommendation 10:

Define Drive Aprons / Sidewalk at Selected Businesses

Provide a definition of drive aprons at selected auto intensive businesses will increase the sense of safety for pedestrians. A discussion with affected property owners of how to define the pedestrian / motorist interaction will occur during the design / construction phase.

Potential pavement options:

- Stamped, textured or colored concrete
- Concrete pavers of contrasting colors

(see fig 2.18)



fig. 2.17 - existing location where pedestrian definition is appropriate



fig. 2.18 - examples of concrete pavers, integral colored concrete and stamped concrete

SECTION 2: GOALS AND RECOMMENDATIONS

Recommendation 11:

Install Bike Racks at Select Locations Within the Corridor

Although not a designated bike route on the City's Major Street Plan, improving the multi-modal environment of the corridor was identified by the public as important. It is recommended that bicycle racks be placed within the nodal areas and that MainCor work with local businesses to encourage installation throughout the corridor.

Bike Rack Considerations:

- Footprint should not impede pedestrian movement.
- The design of the bicycle rack should be simple, clean and functional.
- Placement should be easily accessible to transit riders. (see fig 2.19)



fig. 2.19 - bike rack examples

SECTION 3: CONCEPT PLANS

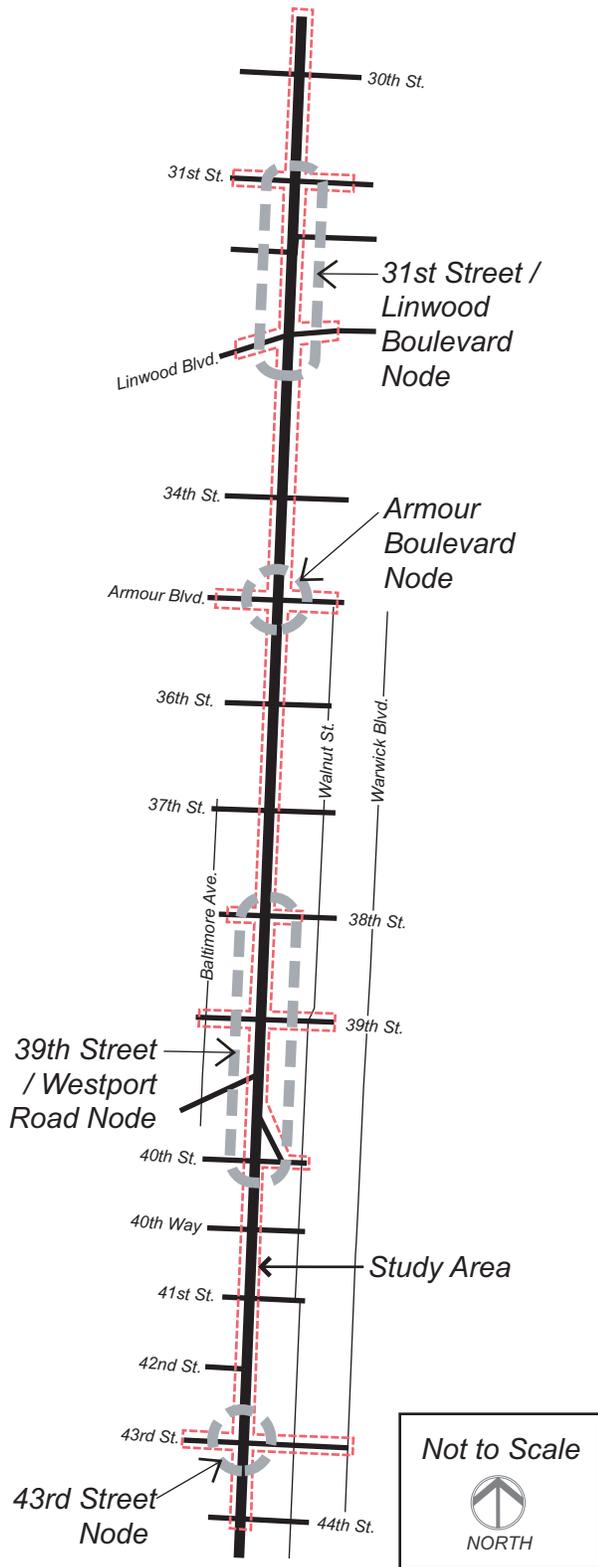


fig. 3.1 - project study area

Nodes Identified and Described

This plan does not propose a uniform treatment or installation of elements throughout the corridor. Instead, improvements are focused where more value can be achieved through focused investment. General recommendations for the four nodes are described below.

Nodal Character - General Recommendations

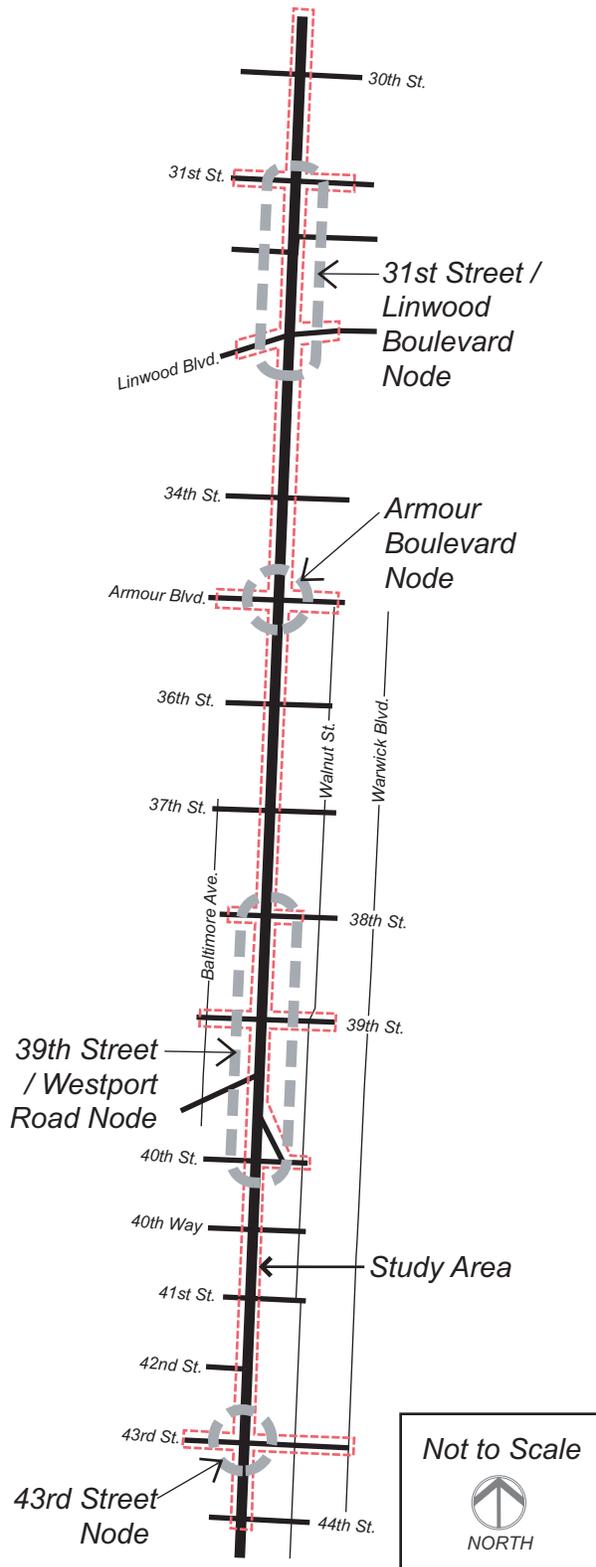
31st Street / Linwood Boulevard

- Install street trees and enhance pedestrian crossings to improve the pedestrian environment.
- Build upon the strength of the historic structures within this node by encouraging similar setbacks or strong edge treatments for new construction projects.
- Establish gateway markers that relate to the area's historic architectural character, at a scale that is appropriate for automobiles.
- Linwood Boulevard is subject to the Parks & Recreation Department's standard boulevard treatment. The character of this boulevard is strong, and any proposed Main Street Corridor Streetscape Master Plan improvements should complement this standard.

Armour Boulevard

- Strengthen urban edges, especially the areas between parking lots and the sidewalk. This improvement will not only provide additional pedestrian security, it will establish parking lot screening and enhance the street line.
- Armour Boulevard is subject to the Parks & Recreation Department's standard boulevard treatment. The character of this boulevard is strong, and any proposed Main Street Corridor Streetscape Master Plan improvements should complement this standard.

SECTION 3: CONCEPT PLANS



39th Street / Westport Road

- Enhance the area’s historic building stock and character through an intensive streetscape treatment that includes street trees, landscaping and pedestrian scale lighting.
- MainCor should work with local partners to improve the connection between the Main Street Corridor and the Westport Entertainment District.

43rd Street

- This node provides important connectivity between Main Street and the area’s art institutions including the Kansas City Art Institute; the Kemper Museum of Contemporary Art and Design; and the Nelson-Atkins Museum of Art. This node also offers connection to the Saint Luke’s Hospital of Kansas City.
- Main Street streetscape treatments in this area shall complement the 43rd and Main Streetscape Concept Plan.
- Incorporate art and artistic elements along sidewalks, creating a unique crosswalk and intersection paving design.

fig. 3.2 - project study area

SECTION 3: CONCEPT PLANS



neighborhood marker



art marker



gateway marker



street light



pedestrian light



street tree



edge treatment



primary crosswalk

Block by Block Concept Plans

The following block by block streetscape plan is meant to be a guide whose components will be adjusted due to factors such as physical limitations and streetscape funded on private property.

Key Streetscape Elements

The following streetscape elements are recommended throughout the corridor and are shown in the following block by block streetscape plan:

- Neighborhood marker
- Art marker
- Gateway marker
- Street light
- Pedestrian light
- Street tree
- Edge treatment
- Primary crosswalk
- Secondary crosswalk

Examples of streetscape elements are shown at the left. The specific streetscape elements will be formalized for the corridor in the initial design / construction streetscape project.



secondary crosswalk

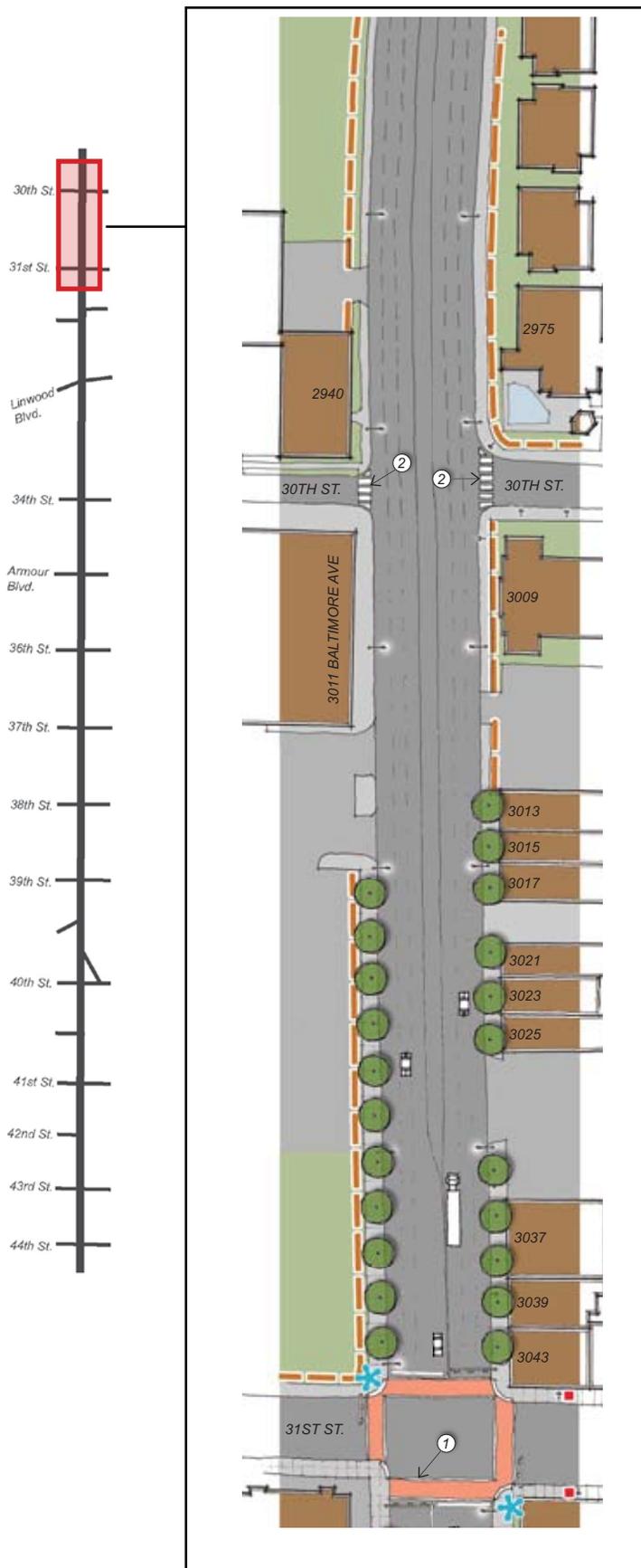
SECTION 3: CONCEPT PLANS

31st Street / Linwood Boulevard Node

30th Street - 31st Street

Proposed streetscape elements:

- Sidewalk width maximized within the existing right of way between curb and property line.
- Street trees (35' on center, min.) where sidewalk is a minimum of 10' wide.
- Edge treatments where buildings are set back from property line.
- Primary crosswalks at 31st Street and Main Street shall incorporate integral colored / stamped concrete to match Main Street corridor standard at the 39th Street intersection.
- Neighborhood markers at selected locations to provide a transition between commercial and residential areas.
- Secondary crosswalk markings at selected locations with properly aligned curb ramps.
- Gateway markers near the intersection of 31st Street and Main Street.



LEGEND

- NEIGHBORHOOD MARKER
- ▲ ART MARKER
- ★ GATEWAY MARKER
- Ⓛ PROPOSED STREET LIGHT
- Ⓛ EXISTING STREET LIGHT
- Ⓛ PROPOSED PEDESTRIAN LIGHT
- Ⓛ EXISTING PEDESTRIAN LIGHT
- BUS STOP
- PROPOSED STREET TREE
- EXISTING STREET TREE
- EDGE TREATMENT
- ① PRIMARY CROSSWALK
- ② SECONDARY CROSSWALK



fig. 3.3 - 30th street - 31st street

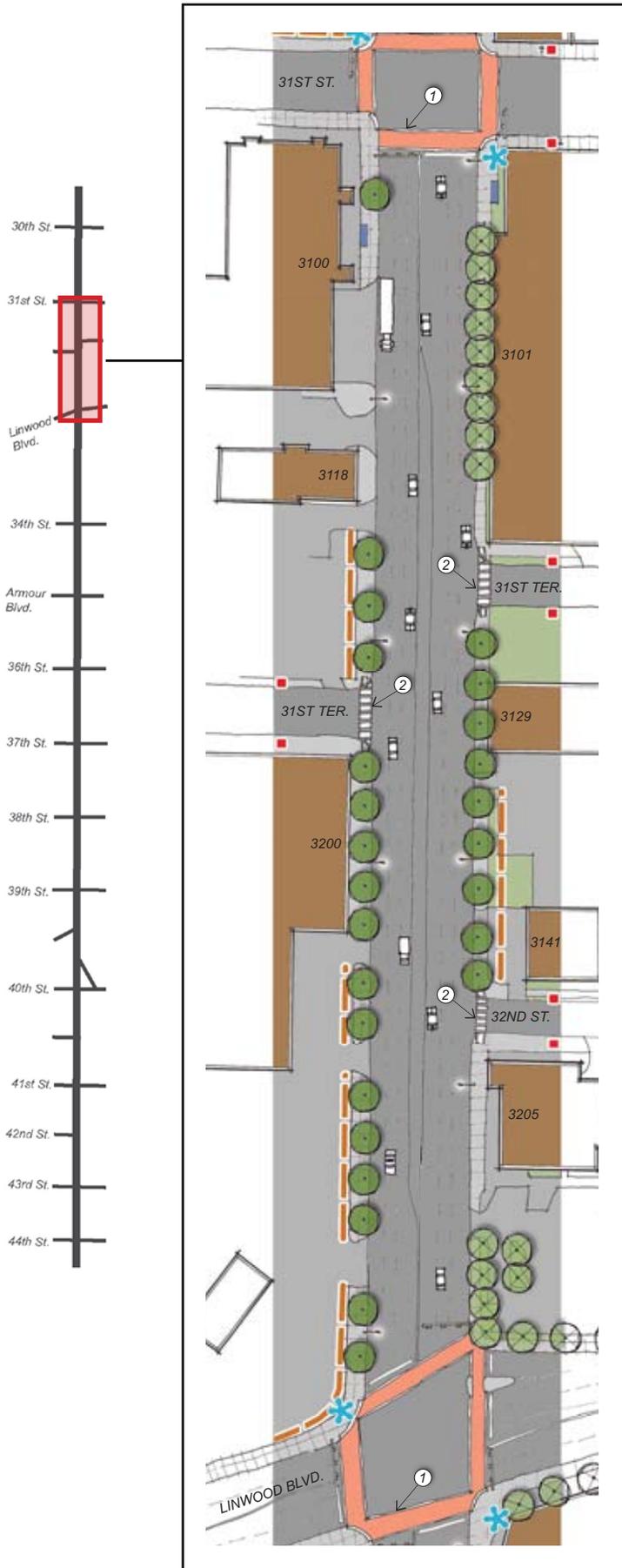
SECTION 3: CONCEPT PLANS

31st Street / Linwood Boulevard Node

31st Street - Linwood Boulevard

Proposed streetscape elements:

- Street trees (35' on center, min.) where sidewalk is a minimum of 10' wide.
- Edge treatments where buildings are set back from property line.
- Primary crosswalks at Linwood Boulevard and Main Street shall incorporate integral colored / stamped concrete to match Main Street corridor standard at the 39th Street intersection.
- Neighborhood markers at selected locations to provide a transition between commercial and residential areas.
- Secondary crosswalk markings at selected locations with properly aligned curb ramps.
- Gateway markers near the intersection of Linwood Boulevard and Main Street.
- Sidewalk width maximized within the existing right of way between curb and property line.



LEGEND

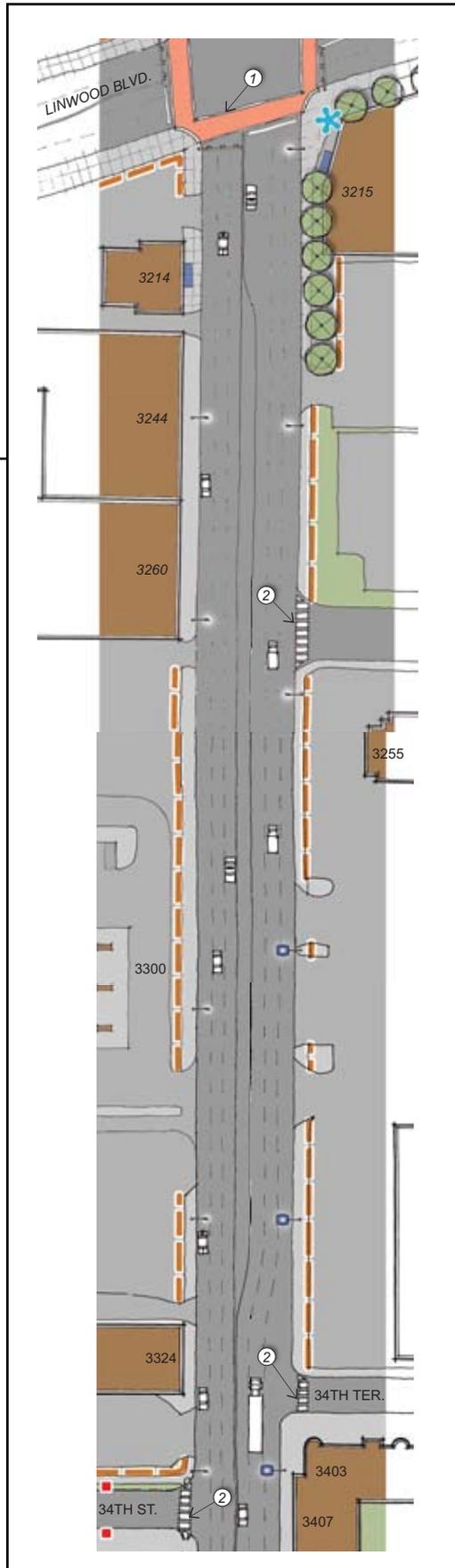
- NEIGHBORHOOD MARKER
- ▲ ART MARKER
- ★ GATEWAY MARKER
- ⬢ PROPOSED STREET LIGHT
- ⬢ EXISTING STREET LIGHT
- ⬢ PROPOSED PEDESTRIAN LIGHT
- ⬢ EXISTING PEDESTRIAN LIGHT
- ⬢ BUS STOP
- PROPOSED STREET TREE
- EXISTING STREET TREE
- EDGE TREATMENT
- ① PRIMARY CROSSWALK
- ② SECONDARY CROSSWALK



fig. 3.4 - 31st street - linwood boulevard

SECTION 3: CONCEPT PLANS

31st Street / Linwood Boulevard Node



Linwood Boulevard - 34th Street

Proposed streetscape elements:

- Edge treatments where buildings are set back from property line.
- Sidewalk width maximized within the existing right of way between curb and property line.
- Secondary crosswalk markings at selected locations with properly aligned curb ramps.
- Street lights on east side of Main Street south of 34th Terrace.
- Neighborhood markers at selected locations to provide a transition between commercial and residential areas.

LEGEND

- NEIGHBORHOOD MARKER
- ▲ ART MARKER
- ★ GATEWAY MARKER
- Ⓛ PROPOSED STREET LIGHT
- Ⓛ EXISTING STREET LIGHT
- Ⓛ PROPOSED PEDESTRIAN LIGHT
- Ⓛ EXISTING PEDESTRIAN LIGHT
- BUS STOP
- PROPOSED STREET TREE
- EXISTING STREET TREE
- EDGE TREATMENT
- ① PRIMARY CROSSWALK
- ② SECONDARY CROSSWALK



fig. 3.5 - linwood boulevard - 34th street

SECTION 3: CONCEPT PLANS

Armour Boulevard Node

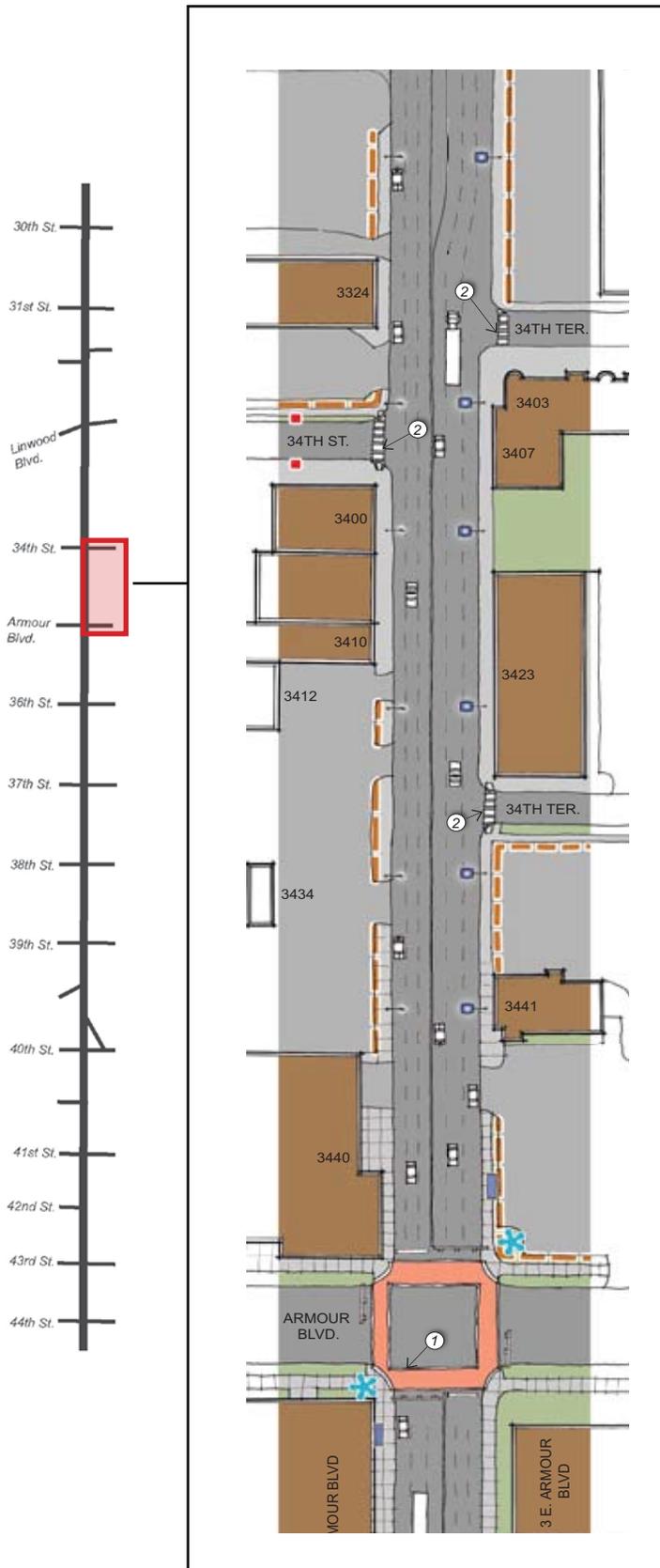
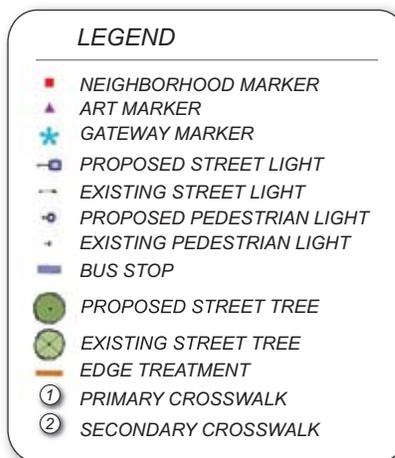


fig. 3.6 - 34th street - armour boulevard

34th Street - Armour Boulevard

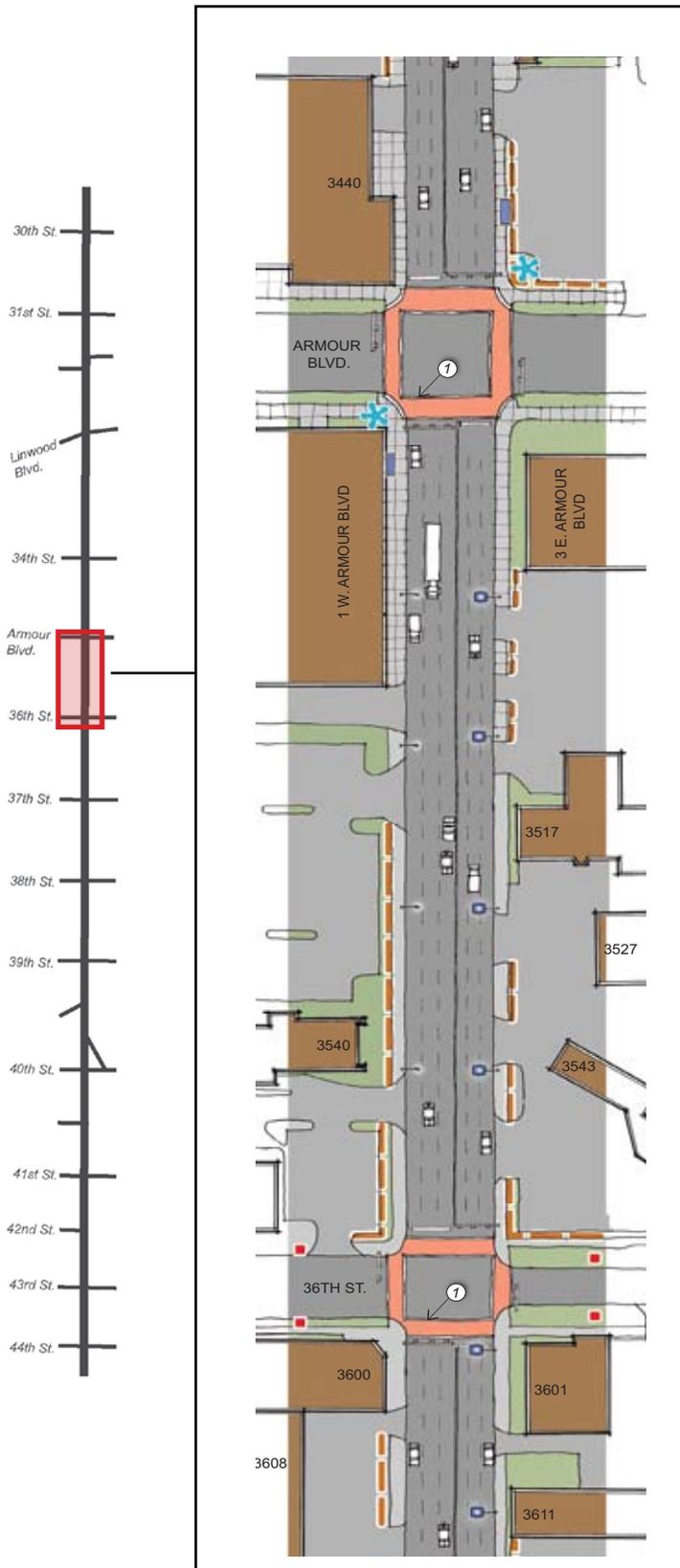
Proposed streetscape elements:

- Edge treatments where buildings are set back from property line.
- Sidewalk width maximized within the existing right of way between curb and property line.
- Secondary crosswalk markings at selected locations with properly aligned curb ramps.
- Street lights on east side of Main Street.
- Gateway markers near intersection of Armour Boulevard and Main Street.
- Primary crosswalks at Armour Boulevard and Main Street shall incorporate integral colored / stamped concrete to match Main Street corridor standard at the 39th Street intersection. **(This improvement is funded by PIAC.)**



SECTION 3: CONCEPT PLANS

Armour Boulevard Node



Armour Boulevard - 36th Street

Proposed streetscape elements:

- Edge treatments where buildings are set back from property line.
- Sidewalk width maximized within the existing right of way between curb and property line.
- Street lights on east side of Main Street.
- Primary crosswalk at 36th Street and Main Street shall incorporate integral colored / stamped concrete to match Main Street corridor standard at the 39th Street intersection.
- Neighborhood markers at selected locations to provide a transition between commercial and residential areas.
- Gateway markers near intersection of Armour Boulevard and Main Street.

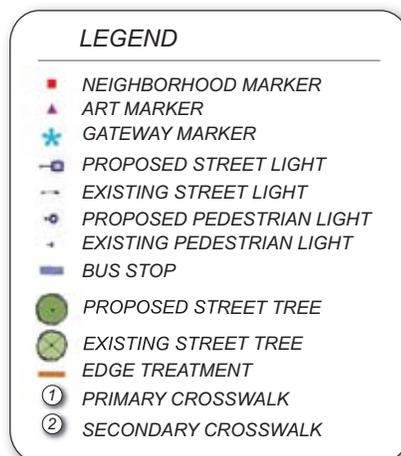


fig. 3.7 - armour boulevard - 36th street



SECTION 3: CONCEPT PLANS

Armour Boulevard Node

36th Street - 37th Street

Proposed streetscape elements:

- Edge treatments where buildings are set back from property line.
- Sidewalk width maximized within the existing right of way between curb and property line.
- Secondary crosswalk markings at selected locations with properly aligned curb ramps.
- Street lights on east side of Main Street.
- Neighborhood markers at selected locations to provide a transition between commercial and residential areas.

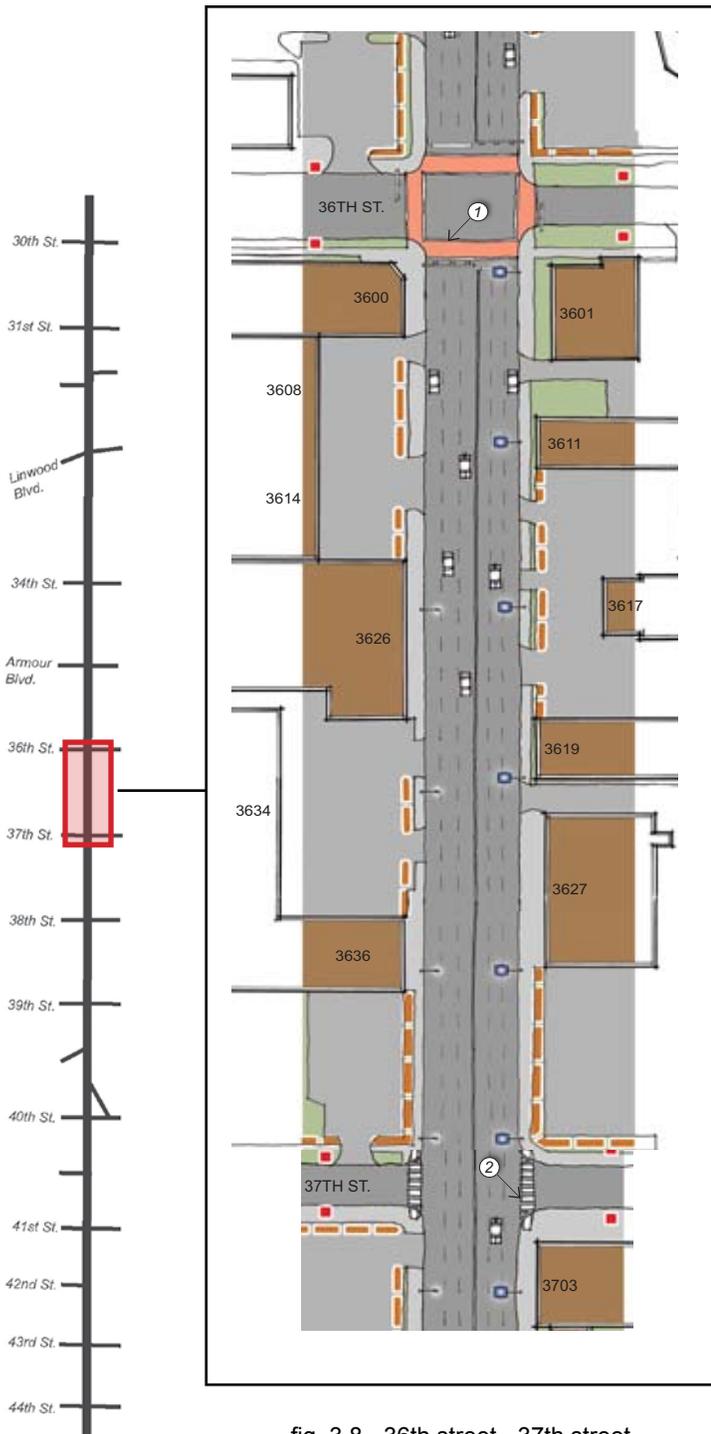


fig. 3.8 - 36th street - 37th street

| LEGEND | |
|--------|---------------------------|
| | NEIGHBORHOOD MARKER |
| | ART MARKER |
| | GATEWAY MARKER |
| | PROPOSED STREET LIGHT |
| | EXISTING STREET LIGHT |
| | PROPOSED PEDESTRIAN LIGHT |
| | EXISTING PEDESTRIAN LIGHT |
| | BUS STOP |
| | PROPOSED STREET TREE |
| | EXISTING STREET TREE |
| | EDGE TREATMENT |
| | PRIMARY CROSSWALK |
| | SECONDARY CROSSWALK |



SECTION 3: CONCEPT PLANS

39th Street / Westport Road Node

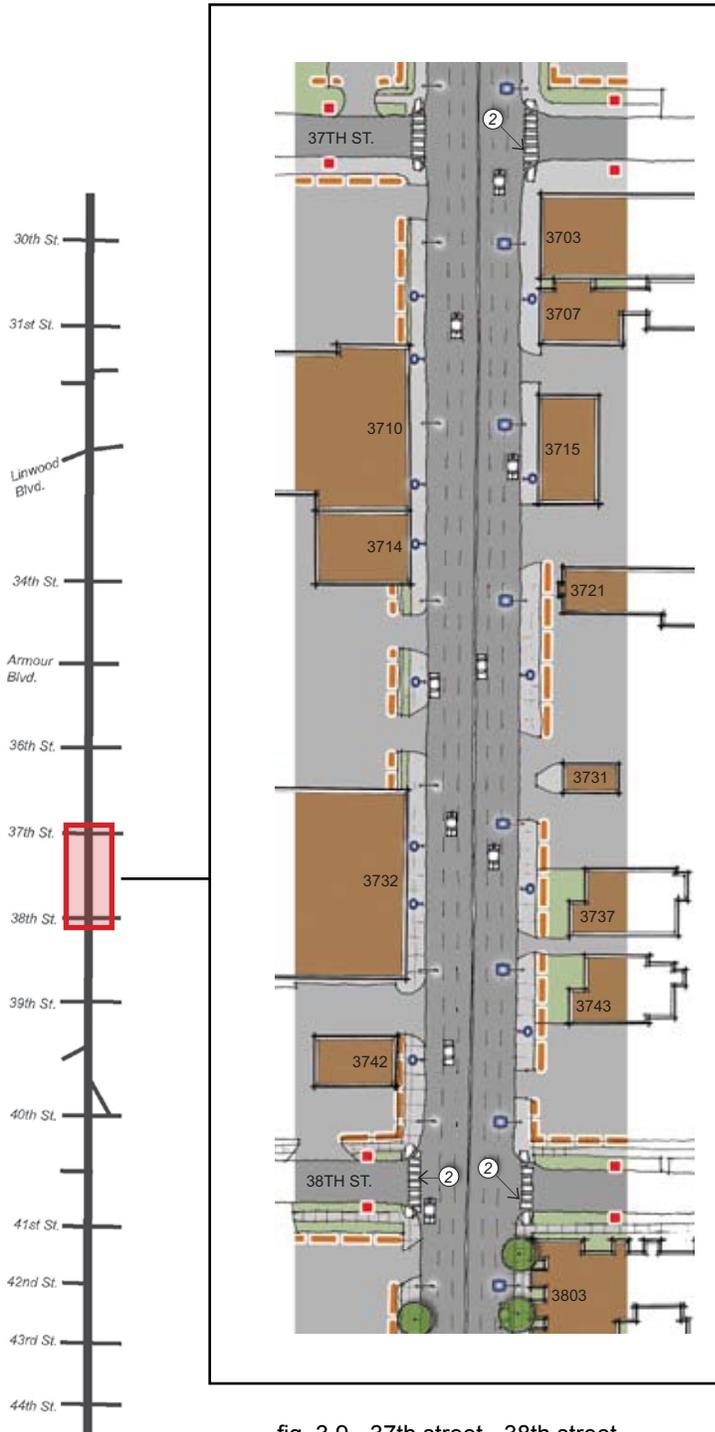


fig. 3.9 - 37th street - 38th street

37th Street - 38th Street

Proposed streetscape elements:

- Pedestrian lights along both sides of Main Street.
- Sidewalk width maximized within the existing right of way between curb and property line.
- Edge treatments where buildings are set back from property line.
- Neighborhood markers at selected locations to provide a transition between commercial and residential areas.
- Secondary crosswalk markings at selected locations with properly aligned curb ramps.
- Street lights on east side of Main Street.
- Crosswalk improvements at the 38th Street and Main Street Intersection. Work with the Public Works Department to explore possible pedestrian improvements at this intersection.

LEGEND

- NEIGHBORHOOD MARKER
- ▲ ART MARKER
- ★ GATEWAY MARKER
- Ⓛ PROPOSED STREET LIGHT
- Ⓛ EXISTING STREET LIGHT
- Ⓛ PROPOSED PEDESTRIAN LIGHT
- Ⓛ EXISTING PEDESTRIAN LIGHT
- BUS STOP
- PROPOSED STREET TREE
- ⊗ EXISTING STREET TREE
- EDGE TREATMENT
- ① PRIMARY CROSSWALK
- ② SECONDARY CROSSWALK



SECTION 3: CONCEPT PLANS

39th Street / Westport Road Node

38th Street - 39th Street

Proposed streetscape elements:

- Pedestrian lights along both sides of Main Street in this block.
- Edge treatments where buildings are set back from property line.
- Secondary crosswalk markings at selected locations with properly aligned curb ramps.
- Street lights on east side of Main Street.
- Street trees (35' on center, min.) where sidewalk is a minimum of 10' wide.
- Gateway markers near the intersection of 39th Street and Main Street.
- Sidewalk width maximized within the existing right of way between curb and property line.

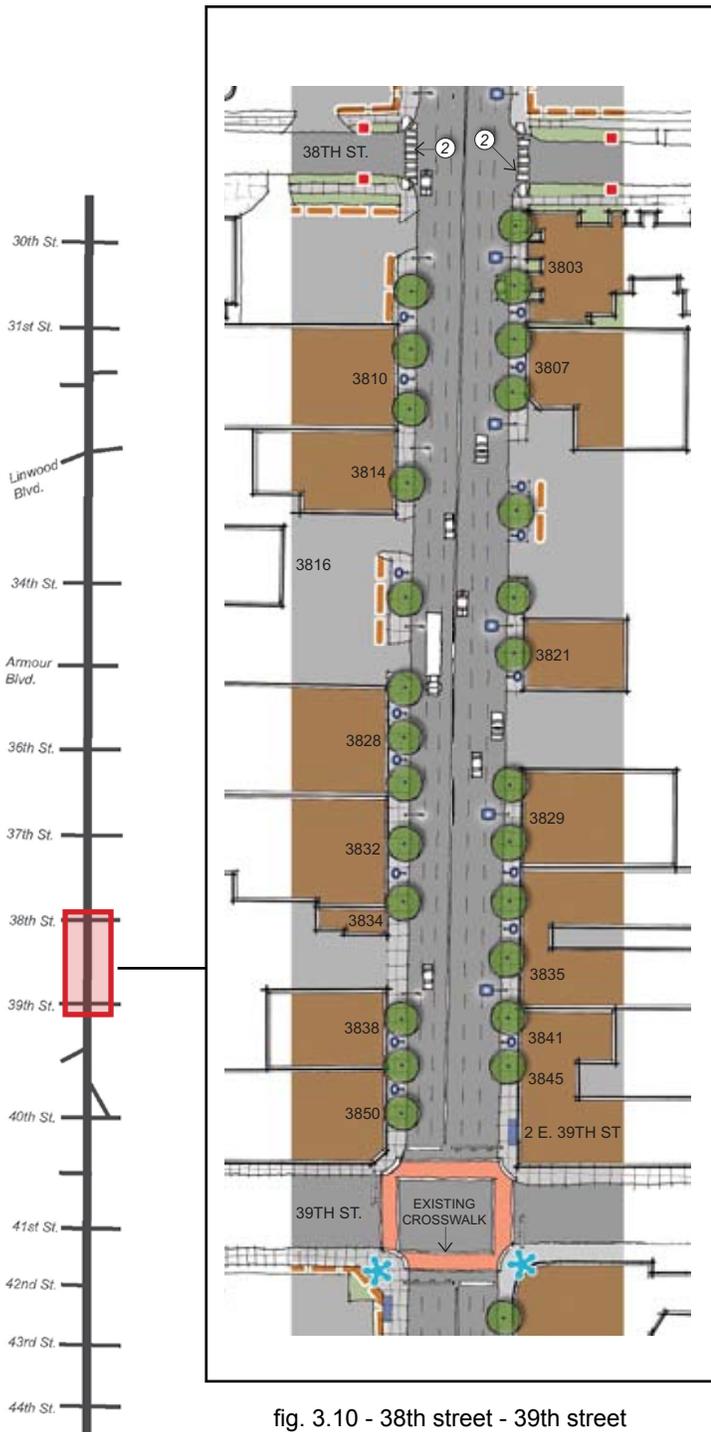


fig. 3.10 - 38th street - 39th street

LEGEND

- NEIGHBORHOOD MARKER
- ▲ ART MARKER
- ★ GATEWAY MARKER
- Ⓛ PROPOSED STREET LIGHT
- Ⓛ EXISTING STREET LIGHT
- Ⓛ PROPOSED PEDESTRIAN LIGHT
- Ⓛ EXISTING PEDESTRIAN LIGHT
- Ⓛ BUS STOP
- PROPOSED STREET TREE
- EXISTING STREET TREE
- EDGE TREATMENT
- ① PRIMARY CROSSWALK
- ② SECONDARY CROSSWALK



SECTION 3: CONCEPT PLANS

39th Street / Westport Road Node

Westport Road - 40th Street

Proposed streetscape elements:

- Pedestrian lights along both sides of Main Street in this block.
- Street lights on east side of Main Street.
- Street trees (35' on center, min.) where sidewalk is a minimum of 10' wide.
- Primary crosswalk at 40th Street and Main Street shall incorporate integral colored / stamped concrete to match Main Street corridor standard at the 39th Street intersection.
- Reconfigure the parking lot at 40th Street and Main Street with expansion of Mnookin Plaza. (see next page)
- Neighborhood markers at selected locations to provide a transition between commercial and residential areas.
- Edge treatments where buildings are set back from property line.
- Secondary crosswalk markings at selected locations with properly aligned curb ramps.
- Gateway markers near the intersection of Westport Road and Main Street.
- Sidewalk width maximized within the existing right of way between curb and property line.

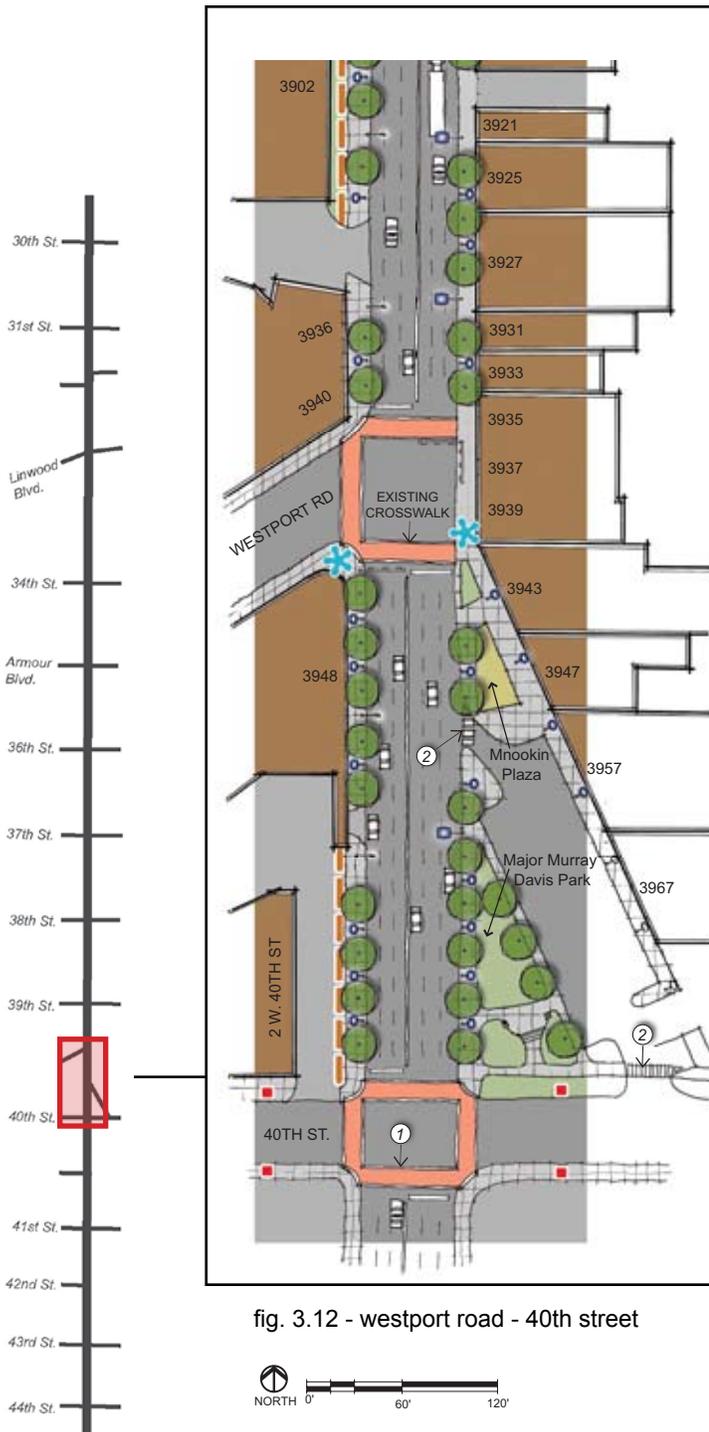


fig. 3.12 - westport road - 40th street



LEGEND

- NEIGHBORHOOD MARKER
- ▲ ART MARKER
- ★ GATEWAY MARKER
- PROPOSED STREET LIGHT
- - - EXISTING STREET LIGHT
- PROPOSED PEDESTRIAN LIGHT
- - - EXISTING PEDESTRIAN LIGHT
- BUS STOP
- PROPOSED STREET TREE
- EXISTING STREET TREE
- EDGE TREATMENT
- ① PRIMARY CROSSWALK
- ② SECONDARY CROSSWALK

SECTION 3: CONCEPT PLANS

39th Street / Westport Road Node

Westport Road - 40th Street



fig. 3.13 - perspective view of 40th street and main street (looking northwest)

This illustration shows the impact of the proposed streetscape improvements at 40th Street and Main Street with a one-way exit only onto Main Street and an expanded Mnookin Plaza. Refer to the previous page for a list of the streetscape elements at this block.



fig. 3.14 - pedestrian view of main street at the former katz drug store (looking north)

This illustration shows the buffering effect of the streetscape elements.

SECTION 3: CONCEPT PLANS

39th Street / Westport Road Node

40th Street - 41st Street

Proposed streetscape elements:

- Pedestrian lights along both sides of Main Street in this block.
- Street lights on east side of Main Street.
- Neighborhood markers at selected locations to provide a transition between commercial and residential areas.
- Edge treatments where buildings are set back from property line.
- Secondary crosswalk markings at selected locations with properly aligned curb ramps.
- Sidewalk width maximized within the existing right of way between curb and property line.

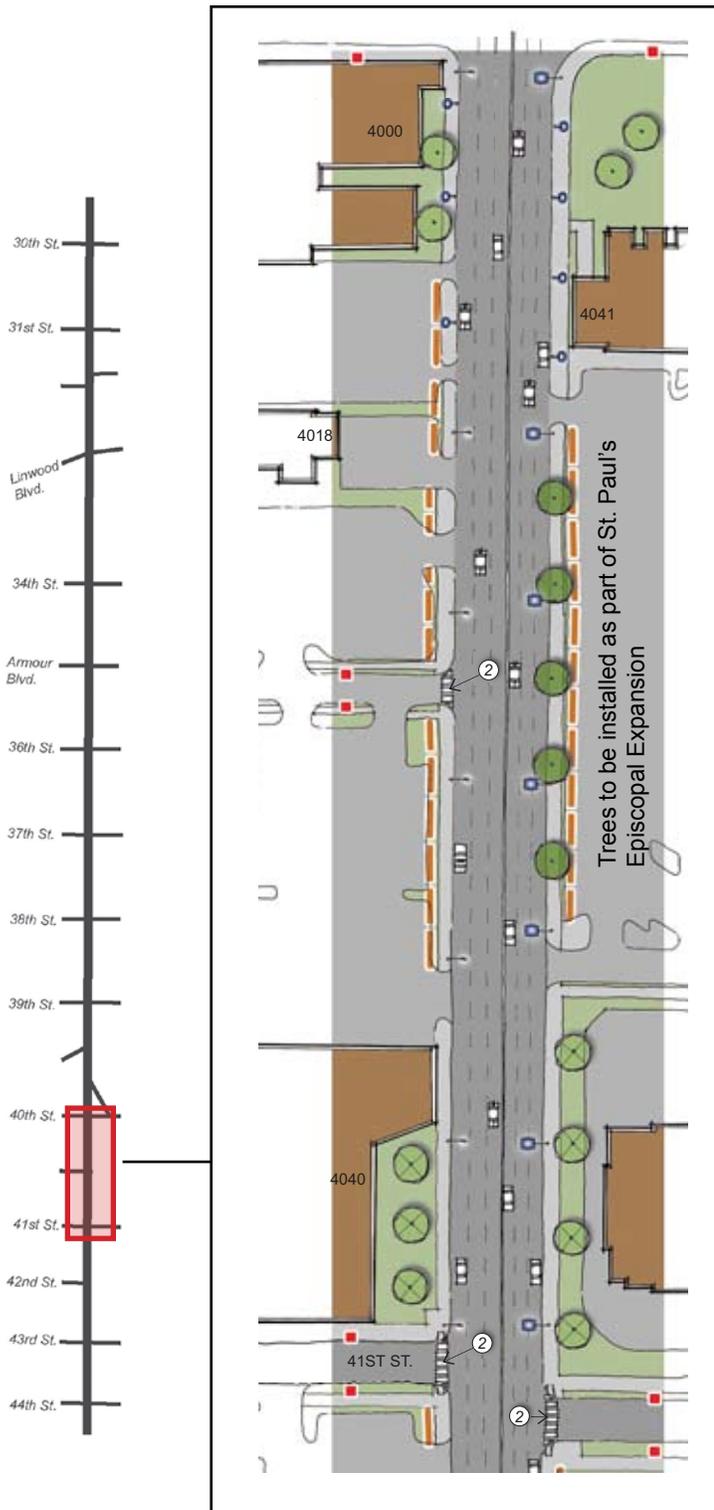
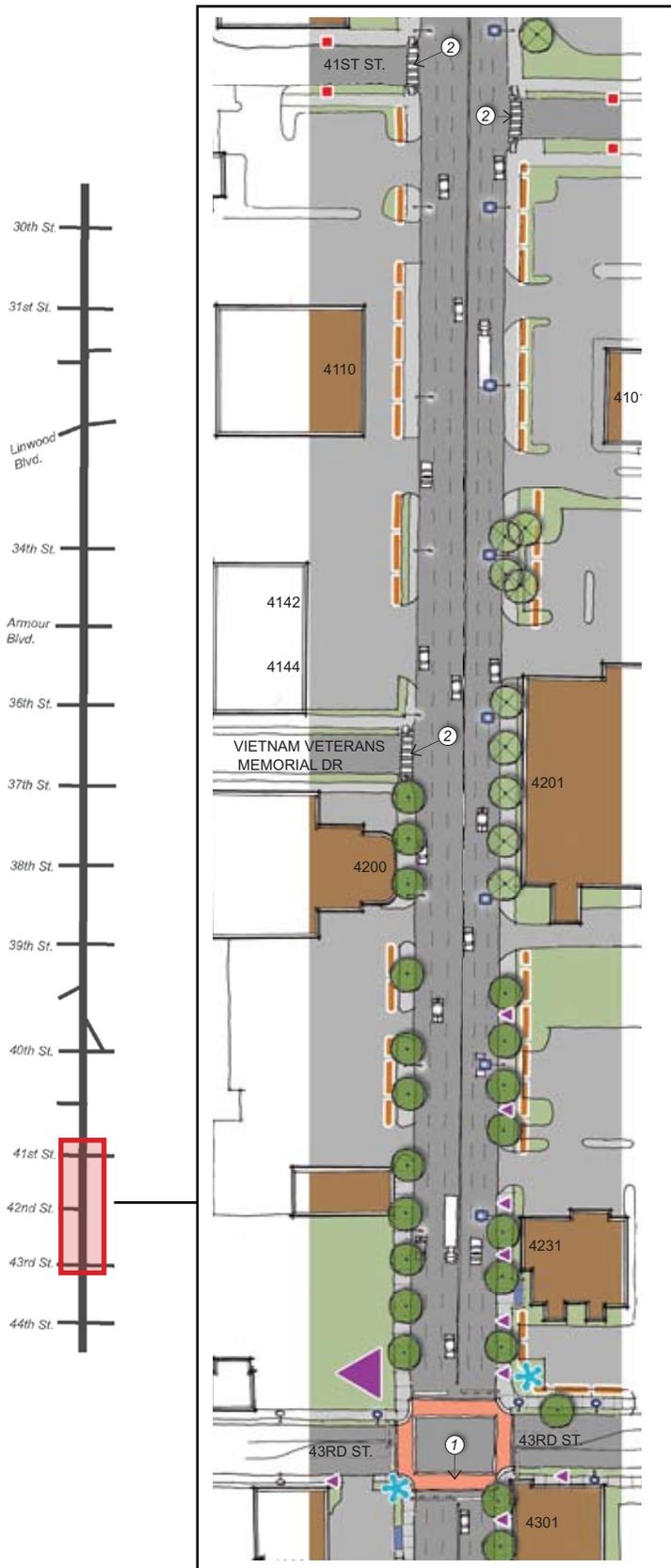


fig. 3.15 - 40th street - 41st street

SECTION 3: CONCEPT PLANS

43rd Street Node



41st Street - 43rd Street

Proposed streetscape elements:

- Art markers on east side of Main Street to identify 43rd Street east of Main Street as a gateway to the Art Institute and museums.
- Street lights on east side of Main Street.
- Street trees (35' on center, min.) where sidewalk is a minimum of 10' wide.
- Neighborhood markers at selected locations to provide a transition between commercial and residential areas.
- Edge treatments where buildings are set back from property line.
- Secondary crosswalk markings at selected locations with properly aligned curb ramps.
- Art piece at northwest corner of 43rd Street and Main Street (temporary until land is developed).
- Gateway markers near intersection of 43rd Street and Main Street.
- Primary crosswalk at 43rd Street and Main Street as designed and approved per the 43rd and Main Streetscape Master Plan.

fig. 3.16 - 41st. street - 43rd. street



LEGEND

- NEIGHBORHOOD MARKER
- ▲ ART MARKER
- ✳ GATEWAY MARKER
- ⬢ PROPOSED STREET LIGHT
- ⬢ EXISTING STREET LIGHT
- ⬢ PROPOSED PEDESTRIAN LIGHT
- ⬢ EXISTING PEDESTRIAN LIGHT
- ⬢ BUS STOP
- PROPOSED STREET TREE
- EXISTING STREET TREE
- EDGE TREATMENT
- ① PRIMARY CROSSWALK
- ② SECONDARY CROSSWALK

SECTION 3: CONCEPT PLANS

43rd Street Node

41st Street - 43rd Street

- Bus shelter on east side of Main Street relocated to original position by 4231 Main Street. Work with KCATA to discuss relocation.
- Sidewalk width maximized within the existing right of way between curb and property line.



fig. 3.17 - perspective view of 43rd street and main street (looking northeast)

This illustration shows the impact of the streetscape elements at this location. Refer to the previous page for a list of the streetscape elements at this block.

SECTION 4: IMPLEMENTATION



fig. 4.1 updated traffic signal at 40th street



fig. 4.2 updated pedestrian crossing activator

Introduction

There is significant positive momentum on Main Street in recent years which seems to indicate a bright future for the corridor. Although this is an ongoing process, since the adoption of the Main Street Corridor Land Use and Development Plan, a number of the plan's action items, including the creation of this plan, have been realized, including:

- Attraction of an increased amount of private development and redevelopment in the corridor.
- Installation of capital improvements with PIAC funding.
- Formation of a Community Improvement District.
- Completion of downzoning the Old Hyde Park Neighborhood from a multi-family residential to single family residential zoning designation.

The following implementation issues will be discussed in detail within this chapter:

- Recommended project phasing and construction.
- Funding opportunities.
- Action steps to achieve this plan's key recommendations.
- Summary cost opinion to associate costs with each project.

SECTION 4: IMPLEMENTATION



fig. 4.3 recommended project areas

Recommended Project Phasing

Phasing a large project into smaller more achievable projects is critical to insure that implementation is initiated. The following is the recommended project phasing established by the Steering Committee during the planning process. (see fig. 4.3)

- Phase 1 - 37th Street - 41st Street**
 This area has the most vacant buildings but also the richest historic building stock in the study area. Public investment in this location will have the most impact to spur future improvement.
- Phase 2 - 41st Street - 44th Street**
 A portion of this project is already funded.
- Phase 3 - 34th Street - 37th Street**
- Phase 4 - 30th Street - 34th Street**

Upon the adoption of this Plan, MainCor will form a committee that is responsible for implementation of the policies of this plan. Their responsibilities will include periodic evaluation of the phasing of the proposed improvements, review and evaluation of future streetscape projects and update of the Main Street Special Review District guidelines.

SECTION 4: IMPLEMENTATION



fig. 4.4 43rd street traffic light construction



fig. 4.5 43rd street traffic light construction

Funding Opportunities

Depending on funding opportunities the phases may be split into smaller projects or combined into larger projects than are defined in this section. Funding for these projects may come from a variety of local, state and federal sources and include:

- Surface Transportation Program - Transportation Enhancement Funds (STP-TE)
- Federal Highway Administration Congestion Mitigation and Air Quality Improvement Program (CMAQ)
- Tax Increment Financing Funds (TIF)
- Kansas City, Missouri Capital Improvements Program (CIP)
- Community Improvement District Funds (CID)
- Public Improvements Advisory Committee Funds (PIAC)
- Funding through partnership with the Kansas City, Missouri Parks and Recreation Department
- Private funding for improvements on private property

(see Appendix C for definition of funding opportunities)

Maintenance

The MainCor Implementation Committee will establish an agreement for the maintenance of streetscape improvements that exceed the City standard. These items typically include pedestrian lights, street trees, decorative plantings and other street furnishings.

SECTION 4: IMPLEMENTATION

Action Steps

The success of any plan is determined by its implementation. The following matrices (fig. 4.6 and fig. 4.7) offer guidance as this project moves from planning to construction. The matrices have 4 components:

- **Actions** defines the necessary measures that need to occur to implement the Main Street Corridor Streetscape Master Plan.
- **Project Types** references who is responsible for implementing the action.
- **Primary Participants** outlines the parties involved with the process.
- **Potential Funding** is a suggested list of funding sources; however this list is not all inclusive as other opportunities may arise.

KEY:

- MC = MainCor
 CID = Main Street Community Improvement District
 SRD = Main Street Special Review District

| Actions | Project Type | | | Primary Participants | Potential Funding |
|--|--------------|--------|--------|---|---------------------|
| | Private | Public | MC/CID | | |
| Public Policy | | | | | |
| Pursue Adoption of the Main Street Corridor Streetscape Master Plan | | X | X | City and MC | No funding required |
| Establish Main Street Streetscape Design Standards through a demonstration project | X | X | X | MC, CID and private property owners | PIAC, MC, CID |
| Update Main Street SRD Design Guidelines | | X | X | City, MC, CID and private property owners | PIAC, MC, CID |
| Enforcement of Streetscape Design Standards and SRD Design Guidelines | X | X | X | City, MC, CID and private property owners | Not Applicable |
| Execute Public/Private Maintenance Agreements | X | X | X | City, MC, CID and private property owners | No funding required |

fig. 4.6 public policy actions

SECTION 4: IMPLEMENTATION

| Actions | Project Type | | | Primary Participants | Potential Funding |
|---|--------------|--------|--------|--|--|
| | Private | Public | MC/CID | | |
| Design / Construction | | | | | |
| Increase Width of Sidewalks | X | X | | MC, City, private property owners | PIAC, CIP, Private Development |
| Update Infrastructure | | X | | MC, City | PIAC, CIP |
| Increase Roadway Lighting | | X | X | MC, City | PIAC, CIP, TIF |
| Consolidate Regulatory Signage | | X | | MC, City | PIAC, CIP |
| Provide Edge Treatments | X | | | SRD,private property owners | Private Development |
| Install Neighborhood Markers | | X | X | City, MC, neighborhood associations | TIF, PIAC |
| Install Gateway Markers | X | X | X | MC, City | Private Development, TIF, PIAC, CMAQ, STP-TE |
| Transform Utility Elements to Visual Assets | | X | X | MC, City | TIF, PIAC, CMAQ, STP-TE |
| Incorporate Street Trees | X | | X | MC, City, CID, private property owners | Private Development, TIF, CMAQ, STP-TE |
| Define Drive Aprons | X | | | MC, private property owners | Private Development, TIF, PIAC, CMAQ, STP-TE |
| Install Bike Racks | X | | X | MC, private property owners | Private Development, TIF, CMAQ, STP-TE |

fig. 4.7 design / construction actions

SECTION 4: IMPLEMENTATION

Cost Opinion

The following is an estimation for comprehensive streetscape improvements within the project area. This is only meant to be a guide; actual project costs will be established on a case by case basis in the design and construction phase of this process.

The following items are included in the cost opinions:

- **Site Preparation:** Mobilization, sidewalk removal, street removal for crosswalks.
- **Paving:** Sidewalks, curbs, crosswalks.
- **Planting:** Street trees, perennials at planters, amended soil to limits of tree planter, 4'x8' concrete planters (tree grates where additional walking surface is needed), mulch, drainage for planters.
- **Crosswalks:** These are assumed to be constructed as part of a larger project such as per block or node development. If the crosswalks are constructed separately the unit cost will likely increase.
- **Streetscape Utilities:** New street lights, pedestrian lights, wiring, controls, conduit, new curb inlets, storm pipe relocation, fire hydrant relocation, pipe reaming and cleaning.
- **Streetscape Structures:** Edge treatments, neighborhood markers, gateway markers, art markers.
- **Misc. Items:** Survey, Arch./Eng. Fees, Construction Management, Contractor Overhead and Profit, and a 20% Contingency to account for unknown items.

SECTION 4: IMPLEMENTATION

Cost Opinion

The following items are excluded from the cost opinions:

- **Street Improvements:** Road surface, curb replacement, utilities within roadway.
- **KCPL Utilities:** Connections, transformer installation, or vault boring. *(A typical utility pole/transformer connection to new controller is no cost if within 200 ft. of new service. Added costs can occur if a new utility transformer is over 200 ft. from the utility connection, or bore holes need to be drilled into an electrical vault or manhole.)*
- **Electrical demolition:** Street lighting, traffic signals, or electrical utilities.
- **Traffic Signals:** Intersection street lighting/traffic signal upgrades.
- **Misc.:** Unforeseen site conditions such as vaults or basements extending underneath sidewalk.

SECTION 4: IMPLEMENTATION

SUMMARY COST OPINION

The following cost opinion reflects implementation of the entire Master Plan. This is a guide only and should be used as such. Construction costs are on the rise due to many factors and it is assumed that the cost of these projects will rise annually. See the summary below for break down by block total and by project total.

SUMMARY BY BLOCK

| Block | Private | Public | Block Total |
|---|------------------------|------------------------|-------------------------|
| 30-31 | \$ 135,000.00 | \$ 704,100.00 | \$ 839,100.00 |
| 31-linwood | \$ 181,500.00 | \$ 715,600.00 | \$ 897,100.00 |
| linwood - 34 | \$ 277,500.00 | \$ 265,100.00 | \$ 542,600.00 |
| 34-Armour | \$ 111,000.00 | \$ 427,800.00 | \$ 538,800.00 |
| Armour-36 | \$ 165,000.00 | \$ 502,200.00 | \$ 667,200.00 |
| 36-37 | \$ 165,000.00 | \$ 332,600.00 | \$ 497,600.00 |
| 37-38 | \$ 154,500.00 | \$ 516,100.00 | \$ 670,600.00 |
| 38-39 | \$ 66,000.00 | \$ 930,100.00 | \$ 996,100.00 |
| 39-Westport | \$ - | \$ 738,400.00 | \$ 738,400.00 |
| Westport-40 *\$100,000 added for Mnookin Plaza Expansion | \$ 45,000.00 | \$ 1,074,600.00 | \$ 1,119,600.00 |
| 40-41 | \$ 105,000.00 | \$ 492,600.00 | \$ 597,600.00 |
| 41-43 *\$25,000 added for large art piece at nw corner of 43rd and Main | \$ 318,000.00 | \$ 979,700.00 | \$ 1,297,700.00 |
| 43-44 | \$ 45,000.00 | \$ 561,000.00 | \$ 606,000.00 |
| Total | \$ 1,768,500.00 | \$ 8,239,900.00 | \$ 10,008,400.00 |

SUMMARY BY PROJECT

| Project | 2007 | 2008 | 2009 | 2010 | 2011 |
|--------------|-------------------------|-------------------------|-------------------------|-------------------------|-------------------------|
| 30-34 | \$ 2,278,800.00 | \$ 2,438,400.00 | \$ 2,609,100.00 | \$ 2,791,800.00 | \$ 2,987,300.00 |
| 34-37 | \$ 1,703,600.00 | \$ 1,822,900.00 | \$ 1,950,600.00 | \$ 2,087,200.00 | \$ 2,233,400.00 |
| 37-41 | \$ 4,122,300.00 | \$ 4,410,900.00 | \$ 4,719,700.00 | \$ 5,050,100.00 | \$ 5,403,700.00 |
| 41-44 | \$ 1,903,700.00 | \$ 2,037,000.00 | \$ 2,179,600.00 | \$ 2,332,200.00 | \$ 2,495,500.00 |
| Total | \$ 10,008,400.00 | \$ 10,709,200.00 | \$ 11,459,000.00 | \$ 12,261,300.00 | \$ 13,119,900.00 |

The above summary of projects assumes a 7% per year inflation rate (2007 \$)

*Note: See Appendix A for: for block by block cost opinions.

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

30TH STREET - 31ST STREET

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|------|------|--------------|---------------|-----------------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 2800 | SF | \$ 2.50 | \$ 7,000.00 | |
| Asphalt Street Removal | 2000 | SF | \$ 5.00 | \$ 10,000.00 | for crosswalk |
| Subtotal | | | | | \$ 69,000.00 |
| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
| PCC Pavement - 5" | 2800 | SF | \$ 5.00 | \$ 14,000.00 | SIDEWALK |
| New PCC Curb and Demo of Old | 100 | LF | \$ 25.00 | \$ 2,500.00 | |
| New Curb Ramp and Demo of Old | 4 | EA | \$ 1,500.00 | \$ 6,000.00 | |
| Integral Colored Concrete Crosswalk | 2000 | SF | \$ 45.00 | \$ 90,000.00 | 10" Thick |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 140 | LF | \$ 10.00 | \$ 1,400.00 | 24" W - 70 LF each |
| Subtotal | | | | | \$ 113,900.00 |
| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
| Street Trees | 22 | EA | \$ 500.00 | \$ 11,000.00 | |
| Amended Soil | 88 | CY | \$ 85.00 | \$ 7,480.00 | 4 CY per Tree |
| Mulch | 6.6 | CY | \$ 75.00 | \$ 495.00 | .3 CY per Tree |
| Perennials (In Tree Planters) | 264 | EA | \$ 15.00 | \$ 3,960.00 | 12 per planter |
| Drainage for Planters | 700 | LF | \$ 15.00 | \$ 10,500.00 | |
| Subtotal | | | | | \$ 33,435.00 |
| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
| Lighting and Electrical | 1 | LS | \$ - | \$ - | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 100,000.00 |
| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
| Edge Treatment | 450 | LF | \$ 300.00 | \$ 135,000.00 | private- not in total |
| Tree Grate | 0 | EA | \$ 3,000.00 | \$ - | incl. frame |
| Concrete Planter | 22 | EA | \$ 1,500.00 | \$ 33,000.00 | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 2 | EA | \$ 10,000.00 | \$ 20,000.00 | incl. footing |
| Gateway Markers | 2 | EA | \$ 50,000.00 | \$ 100,000.00 | incl. footing |
| Art Markers | 0 | EA | \$ 10,000.00 | \$ - | incl. footing |
| Subtotal | | | | | \$ 153,000.00 |
| Subtotal | | | | | \$ 469,335.00 |
| Survey (2%) | | | | | \$ 9,386.70 |
| Architectural / Engineering Fees (8%) | | | | | \$ 37,546.80 |
| Construction Management (10%) | | | | | \$ 46,933.50 |
| Contractor Overhead & Profit (10%) | | | | | \$ 46,933.50 |
| Contingency (20%) | | | | | \$ 93,867.00 |
| Total | | | | | \$ 704,002.50 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

31ST STREET - LINWOOD BOULEVARD

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|-------------------------------|------|------|--------------|--------------|---------------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 3400 | SF | \$ 2.50 | \$ 8,500.00 | |
| Asphalt Street Removal | 0 | SF | \$ 5.00 | \$ - | Funded not in total |
| Subtotal | | | | | \$ 60,500.00 |

| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|------|------|-------------|--------------|---------------------|
| PCC Pavement - 5" | 3400 | SF | \$ 5.00 | \$ 17,000.00 | SIDEWALK |
| New PCC Curb and Demo of Old | 200 | LF | \$ 25.00 | \$ 5,000.00 | |
| New Curb Ramp and Demo of Old | 10 | EA | \$ 1,500.00 | \$ 15,000.00 | |
| Integral Colored Concrete Crosswalk | 0 | SF | \$ 45.00 | \$ - | Funded not in total |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 210 | LF | \$ 10.00 | \$ 2,100.00 | 24" W - 70 LF each |
| Subtotal | | | | | \$ 39,100.00 |

| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
|-------------------------------|-----|------|-----------|--------------|----------------|
| Street Trees | 25 | EA | \$ 500.00 | \$ 12,500.00 | |
| Amended Soil | 100 | CY | \$ 85.00 | \$ 8,500.00 | 4 CY per Tree |
| Mulch | 7.5 | CY | \$ 75.00 | \$ 562.50 | .3 CY per Tree |
| Perennials (In Tree Planters) | 300 | EA | \$ 15.00 | \$ 4,500.00 | 12 per planter |
| Drainage for Planters | 850 | LF | \$ 15.00 | \$ 12,750.00 | |
| Subtotal | | | | | \$ 38,812.50 |

| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
|-----------------------------------|-----|------|--------------|--------------|---------------|
| Lighting and Electrical | 1 | LS | \$ 41,103.00 | \$ 41,103.00 | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 141,103.00 |

| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
|------------------------|-----|------|--------------|---------------|-----------------------|
| Edge Treatment | 605 | LF | \$ 300.00 | \$ 181,500.00 | private- not in total |
| Tree Grate | 0 | EA | \$ 3,000.00 | \$ - | incl. frame |
| Concrete Planter | 25 | EA | \$ 1,500.00 | \$ 37,500.00 | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 6 | EA | \$ 10,000.00 | \$ 60,000.00 | incl. footing |
| Gateway Markers | 2 | EA | \$ 50,000.00 | \$ 100,000.00 | incl. footing |
| Art Markers | 0 | EA | \$ 10,000.00 | \$ - | incl. footing |
| Subtotal | | | | | \$ 197,500.00 |

| | | | | | |
|---------------------------------------|--|--|--|--|---------------|
| Subtotal | | | | | \$ 477,015.50 |
| Survey (2%) | | | | | \$ 9,540.31 |
| Architectural / Engineering Fees (8%) | | | | | \$ 38,161.24 |
| Construction Management (10%) | | | | | \$ 47,701.55 |
| Contractor Overhead & Profit (10%) | | | | | \$ 47,701.55 |
| Contingency (20%) | | | | | \$ 95,403.10 |
| Total | | | | | \$ 715,523.25 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

LINWOOD BOULEVARD - 34TH STREET

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|------|------|--------------|---------------|-----------------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 0 | SF | \$ 2.50 | \$ - | |
| Asphalt Street Removal | 0 | SF | \$ 5.00 | \$ - | for crosswalk |
| Subtotal | | | | | \$ 52,000.00 |
| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
| PCC Pavement - 5" | 0 | SF | \$ 5.00 | \$ - | SIDEWALK |
| New PCC Curb and Demo of Old | 0 | LF | \$ 25.00 | \$ - | |
| New Curb Ramp and Demo of Old | 6 | EA | \$ 1,500.00 | \$ 9,000.00 | |
| Integral Colored Concrete Crosswalk | 0 | SF | \$ 45.00 | \$ - | 10" Thick |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 210 | LF | \$ 10.00 | \$ 2,100.00 | 24" W - 70 LF each |
| Subtotal | | | | | \$ 11,100.00 |
| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
| Street Trees | 0 | EA | \$ 500.00 | \$ - | |
| Amended Soil | 0 | CY | \$ 85.00 | \$ - | 4 CY per Tree |
| Mulch | 0 | CY | \$ 75.00 | \$ - | .3 CY per Tree |
| Perennials (In Tree Planters) | 0 | EA | \$ 15.00 | \$ - | 12 per planter |
| Drainage for Planters | 0 | LF | \$ 15.00 | \$ - | |
| Subtotal | | | | | \$ - |
| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
| Lighting and Electrical | 1 | LS | \$ 3,571.80 | \$ 3,571.80 | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 103,571.80 |
| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
| Edge Treatment | 925 | LF | \$ 300.00 | \$ 277,500.00 | private- not in total |
| Tree Grate | 0 | EA | \$ 3,000.00 | \$ - | incl. frame |
| Concrete Planter | 0 | EA | \$ 1,500.00 | \$ - | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 1 | EA | \$ 10,000.00 | \$ 10,000.00 | incl. footing |
| Gateway Markers | 0 | EA | \$ 50,000.00 | \$ - | incl. footing |
| Art Markers | 0 | EA | \$ 10,000.00 | \$ - | incl. footing |
| Subtotal | | | | | \$ 10,000.00 |
| Subtotal | | | | | \$ 176,671.80 |
| Survey (2%) | | | | | \$ 3,533.44 |
| Architectural / Engineering Fees (8%) | | | | | \$ 14,133.74 |
| Construction Management (10%) | | | | | \$ 17,667.18 |
| Contractor Overhead & Profit (10%) | | | | | \$ 17,667.18 |
| Contingency (20%) | | | | | \$ 35,334.36 |
| Total | | | | | \$ 265,007.70 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

34TH STREET - ARMOUR BOULEVARD

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|------|------|--------------|---------------|-----------------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 0 | SF | \$ 2.50 | \$ - | |
| Asphalt Street Removal | 0 | SF | \$ 5.00 | \$ - | Funded not in total |
| Subtotal | | | | | \$ 52,000.00 |
| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
| PCC Pavement - 5" | 0 | SF | \$ 5.00 | \$ - | SIDEWALK |
| New PCC Curb and Demo of Old | 0 | LF | \$ 25.00 | \$ - | |
| New Curb Ramp and Demo of Old | 3 | EA | \$ 1,500.00 | \$ 4,500.00 | |
| Integral Colored Concrete Crosswalk | | SF | \$ 45.00 | \$ - | Funded not in total |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 70 | LF | \$ 10.00 | \$ 700.00 | 24" W - 70 LF each |
| Subtotal | | | | | \$ 5,200.00 |
| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
| Street Trees | 0 | EA | \$ 500.00 | \$ - | |
| Amended Soil | 0 | CY | \$ 85.00 | \$ - | 4 CY per Tree |
| Mulch | 0 | CY | \$ 75.00 | \$ - | .3 CY per Tree |
| Perennials (In Tree Planters) | 0 | EA | \$ 15.00 | \$ - | 12 per planter |
| Drainage for Planters | 0 | LF | \$ 15.00 | \$ - | |
| Subtotal | | | | | \$ - |
| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
| Lighting and Electrical | 1 | LS | \$ 17,961.05 | \$ 17,961.05 | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 117,961.05 |
| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
| Edge Treatment | 370 | LF | \$ 300.00 | \$ 111,000.00 | private- not in total |
| Tree Grate | 0 | EA | \$ 3,000.00 | \$ - | incl. frame |
| Concrete Planter | 0 | EA | \$ 1,500.00 | \$ - | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 1 | EA | \$ 10,000.00 | \$ 10,000.00 | incl. footing |
| Gateway Markers | 2 | EA | \$ 50,000.00 | \$ 100,000.00 | incl. footing |
| Art Markers | 0 | EA | \$ 10,000.00 | \$ - | incl. footing |
| Subtotal | | | | | \$ 110,000.00 |
| Subtotal | | | | | \$ 285,161.05 |
| Survey (2%) | | | | | \$ 5,703.22 |
| Architectural / Engineering Fees (8%) | | | | | \$ 22,812.88 |
| Construction Management (10%) | | | | | \$ 28,516.11 |
| Contractor Overhead & Profit (10%) | | | | | \$ 28,516.11 |
| Contingency (20%) | | | | | \$ 57,032.21 |
| Total | | | | | \$ 427,741.58 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

ARMOUR BOULEVARD - 36TH STREET

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|------|------|--------------|---------------|-----------------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 1000 | SF | \$ 2.50 | \$ 2,500.00 | |
| Asphalt Street Removal | 2200 | SF | \$ 5.00 | \$ 11,000.00 | for crosswalk |
| Subtotal | | | | | \$ 65,500.00 |
| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
| PCC Pavement - 5" | 4000 | SF | \$ 5.00 | \$ 20,000.00 | SIDEWALK |
| New PCC Curb and Demo of Old | 0 | LF | \$ 25.00 | \$ - | |
| New Curb Ramp and Demo of Old | 2 | EA | \$ 1,500.00 | \$ 3,000.00 | |
| Integral Colored Concrete Crosswalk | 2200 | SF | \$ 45.00 | \$ 99,000.00 | 10" Thick |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 0 | LF | \$ 10.00 | \$ - | 24" W - 70 LF each |
| Subtotal | | | | | \$ 122,000.00 |
| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
| Street Trees | 0 | EA | \$ 500.00 | \$ - | |
| Amended Soil | 0 | CY | \$ 85.00 | \$ - | 4 CY per Tree |
| Mulch | 0 | CY | \$ 75.00 | \$ - | .3 CY per Tree |
| Perennials (In Tree Planters) | 0 | EA | \$ 15.00 | \$ - | 12 per planter |
| Drainage for Planters | 0 | LF | \$ 15.00 | \$ - | |
| Subtotal | | | | | \$ - |
| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
| Lighting and Electrical | 1 | LS | \$ 27,250.30 | \$ 27,250.30 | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 127,250.30 |
| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
| Edge Treatment | 550 | LF | \$ 300.00 | \$ 165,000.00 | private- not in total |
| Tree Grate | 0 | EA | \$ 3,000.00 | \$ - | incl. frame |
| Concrete Planter | 0 | EA | \$ 1,500.00 | \$ - | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 2 | EA | \$ 10,000.00 | \$ 20,000.00 | incl. footing |
| Gateway Markers | 0 | EA | \$ 50,000.00 | \$ - | incl. footing |
| Art Markers | 0 | EA | \$ 10,000.00 | \$ - | incl. footing |
| Subtotal | | | | | \$ 20,000.00 |
| Subtotal | | | | | \$ 334,750.30 |
| Survey (2%) | | | | | \$ 6,695.01 |
| Architectural / Engineering Fees (8%) | | | | | \$ 26,780.02 |
| Construction Management (10%) | | | | | \$ 33,475.03 |
| Contractor Overhead & Profit (10%) | | | | | \$ 33,475.03 |
| Contingency (20%) | | | | | \$ 66,950.06 |
| Total | | | | | \$ 502,125.45 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

36TH STREET - 37TH STREET

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|-------------------------------|------|------|--------------|--------------|---------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 0 | SF | \$ 2.50 | \$ - | |
| Asphalt Street Removal | 0 | SF | \$ 5.00 | \$ - | for crosswalk |
| Subtotal | | | | | \$ 52,000.00 |

| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|-----|------|-------------|-------------|--------------------|
| PCC Pavement - 5" | 0 | SF | \$ 5.00 | \$ - | SIDEWALK |
| New PCC Curb and Demo of Old | 0 | LF | \$ 25.00 | \$ - | |
| New Curb Ramp and Demo of Old | 4 | EA | \$ 1,500.00 | \$ 6,000.00 | |
| Integral Colored Concrete Crosswalk | 0 | SF | \$ 45.00 | \$ - | 10" Thick |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 140 | LF | \$ 10.00 | \$ 1,400.00 | 24" W - 70 LF each |
| Subtotal | | | | | \$ 7,400.00 |

| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
|-------------------------------|-----|------|-----------|------------|----------------|
| Street Trees | 0 | EA | \$ 500.00 | \$ - | |
| Amended Soil | 0 | CY | \$ 85.00 | \$ - | 4 CY per Tree |
| Mulch | 0 | CY | \$ 75.00 | \$ - | .3 CY per Tree |
| Perennials (In Tree Planters) | 0 | EA | \$ 15.00 | \$ - | 12 per planter |
| Drainage for Planters | 0 | LF | \$ 15.00 | \$ - | |
| Subtotal | | | | | \$ - |

| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
|-----------------------------------|-----|------|--------------|--------------|---------------|
| Lighting and Electrical | 1 | LS | \$ 22,291.00 | \$ 22,291.00 | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 122,291.00 |

| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
|------------------------|-----|------|--------------|---------------|-----------------------|
| Edge Treatment | 550 | LF | \$ 300.00 | \$ 165,000.00 | private- not in total |
| Tree Grate | 0 | EA | \$ 3,000.00 | \$ - | incl. frame |
| Concrete Planter | 0 | EA | \$ 1,500.00 | \$ - | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 4 | EA | \$ 10,000.00 | \$ 40,000.00 | incl. footing |
| Gateway Markers | 0 | EA | \$ 50,000.00 | \$ - | incl. footing |
| Art Markers | 0 | EA | \$ 10,000.00 | \$ - | incl. footing |
| Subtotal | | | | | \$ 40,000.00 |

| | | | | | |
|---------------------------------------|--|--|--|--|----------------------|
| Subtotal | | | | | \$ 221,691.00 |
| Survey (2%) | | | | | \$ 4,433.82 |
| Architectural / Engineering Fees (8%) | | | | | \$ 17,735.28 |
| Construction Management (10%) | | | | | \$ 22,169.10 |
| Contractor Overhead & Profit (10%) | | | | | \$ 22,169.10 |
| Contingency (20%) | | | | | \$ 44,338.20 |
| Total | | | | | \$ 332,536.50 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

37TH STREET - 38ST STREET

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|------|------|---------------|---------------|-----------------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 5000 | SF | \$ 2.50 | \$ 12,500.00 | |
| Asphalt Street Removal | 0 | SF | \$ 5.00 | \$ - | for crosswalk |
| Subtotal | | | | | \$ 64,500.00 |
| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
| PCC Pavement - 5" | 5000 | SF | \$ 5.00 | \$ 25,000.00 | SIDEWALK |
| New PCC Curb and Demo of Old | 0 | LF | \$ 25.00 | \$ - | |
| New Curb Ramp and Demo of Old | 4 | EA | \$ 1,500.00 | \$ 6,000.00 | |
| Integral Colored Concrete Crosswalk | 0 | SF | \$ 45.00 | \$ - | 10" Thick |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 140 | LF | \$ 10.00 | \$ 1,400.00 | 24" W - 70 LF each |
| Subtotal | | | | | \$ 32,400.00 |
| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
| Street Trees | 0 | EA | \$ 500.00 | \$ - | |
| Amended Soil | 0 | CY | \$ 85.00 | \$ - | 4 CY per Tree |
| Mulch | 0 | CY | \$ 75.00 | \$ - | .3 CY per Tree |
| Perennials (In Tree Planters) | 0 | EA | \$ 15.00 | \$ - | 12 per planter |
| Drainage for Planters | 0 | LF | \$ 15.00 | \$ - | |
| Subtotal | | | | | \$ - |
| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
| Lighting and Electrical | 1 | LS | \$ 107,111.83 | \$ 107,111.83 | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 207,111.83 |
| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
| Edge Treatment | 515 | LF | \$ 300.00 | \$ 154,500.00 | private- not in total |
| Tree Grate | 0 | EA | \$ 3,000.00 | \$ - | incl. frame |
| Concrete Planter | 0 | EA | \$ 1,500.00 | \$ - | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 4 | EA | \$ 10,000.00 | \$ 40,000.00 | incl. footing |
| Gateway Markers | 0 | EA | \$ 50,000.00 | \$ - | incl. footing |
| Art Markers | 0 | EA | \$ 10,000.00 | \$ - | incl. footing |
| Subtotal | | | | | \$ 40,000.00 |
| Subtotal | | | | | \$ 344,011.83 |
| Survey (2%) | | | | | \$ 6,880.24 |
| Architectural / Engineering Fees (8%) | | | | | \$ 27,520.95 |
| Construction Management (10%) | | | | | \$ 34,401.18 |
| Contractor Overhead & Profit (10%) | | | | | \$ 34,401.18 |
| Contingency (20%) | | | | | \$ 68,802.37 |
| Total | | | | | \$ 516,017.75 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

38TH STREET - 39TH STREET

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|-------|------|---------------|---------------|-----------------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 14000 | SF | \$ 2.50 | \$ 35,000.00 | |
| Asphalt Street Removal | | SF | \$ 5.00 | \$ - | for crosswalk |
| Subtotal | | | | | \$ 87,000.00 |
| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
| PCC Pavement - 5" | 14000 | SF | \$ 5.00 | \$ 70,000.00 | SIDEWALK |
| New PCC Curb and Demo of Old | 0 | LF | \$ 25.00 | \$ - | |
| New Curb Ramp and Demo of Old | 4 | EA | \$ 1,500.00 | \$ 6,000.00 | |
| Integral Colored Concrete Crosswalk | | SF | \$ 45.00 | \$ - | 10" Thick |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 0 | LF | \$ 10.00 | \$ - | 24" W - 70 LF each |
| Subtotal | | | | | \$ 76,000.00 |
| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
| Street Trees | 26 | EA | \$ 500.00 | \$ 13,000.00 | |
| Amended Soil | 104 | CY | \$ 85.00 | \$ 8,840.00 | 4 CY per Tree |
| Mulch | 7.8 | CY | \$ 75.00 | \$ 585.00 | .3 CY per Tree |
| Perennials (In Tree Planters) | 144 | EA | \$ 15.00 | \$ 2,160.00 | 12 per planter |
| Drainage for Planters | 1040 | LF | \$ 15.00 | \$ 15,600.00 | |
| Subtotal | | | | | \$ 40,185.00 |
| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
| Lighting and Electrical | 1 | LS | \$ 136,818.00 | \$ 136,818.00 | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 236,818.00 |
| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
| Edge Treatment | 220 | LF | \$ 300.00 | \$ 66,000.00 | private- not in total |
| Tree Grate | 14 | EA | \$ 3,000.00 | \$ 42,000.00 | incl. frame |
| Concrete Planter | 12 | EA | \$ 1,500.00 | \$ 18,000.00 | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 2 | EA | \$ 10,000.00 | \$ 20,000.00 | incl. footing |
| Gateway Markers | 2 | EA | \$ 50,000.00 | \$ 100,000.00 | incl. footing |
| Art Markers | 0 | EA | \$ 10,000.00 | \$ - | incl. footing |
| Subtotal | | | | | \$ 180,000.00 |
| Subtotal | | | | | \$ 620,003.00 |
| Survey (2%) | | | | | \$ 12,400.06 |
| Architectural / Engineering Fees (8%) | | | | | \$ 49,600.24 |
| Construction Management (10%) | | | | | \$ 62,000.30 |
| Contractor Overhead & Profit (10%) | | | | | \$ 62,000.30 |
| Contingency (20%) | | | | | \$ 124,000.60 |
| Total | | | | | \$ 930,004.50 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

39TH STREET - WESTPORT ROAD

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|-------|------|---------------|---------------|-----------------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 10600 | SF | \$ 2.50 | \$ 26,500.00 | |
| Asphalt Street Removal | 0 | SF | \$ 5.00 | \$ - | for crosswalk |
| Subtotal | | | | | \$ 78,500.00 |
| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
| PCC Pavement - 5" | 10600 | SF | \$ 5.00 | \$ 53,000.00 | SIDEWALK |
| New PCC Curb and Demo of Old | 0 | LF | \$ 25.00 | \$ - | |
| New Curb Ramp and Demo of Old | 4 | EA | \$ 1,500.00 | \$ 6,000.00 | |
| Integral Colored Concrete Crosswalk | 0 | SF | \$ 45.00 | \$ - | 10" Thick |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 0 | LF | \$ 10.00 | \$ - | 24" W - 70 LF each |
| Subtotal | | | | | \$ 59,000.00 |
| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
| Street Trees | 16 | EA | \$ 500.00 | \$ 8,000.00 | |
| Amended Soil | 64 | CY | \$ 85.00 | \$ 5,440.00 | 4 CY per Tree |
| Mulch | 4.8 | CY | \$ 75.00 | \$ 360.00 | .3 CY per Tree |
| Perennials (In Tree Planters) | 192 | EA | \$ 15.00 | \$ 2,880.00 | 12 per planter |
| Drainage for Planters | 620 | LF | \$ 15.00 | \$ 9,300.00 | |
| Subtotal | | | | | \$ 25,980.00 |
| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
| Lighting and Electrical | 1 | LS | \$ 104,737.20 | \$ 104,737.20 | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 204,737.20 |
| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
| Edge Treatment | 0 | LF | \$ 300.00 | \$ - | private- not in total |
| Tree Grate | 0 | EA | \$ 3,000.00 | \$ - | incl. frame |
| Concrete Planter | 16 | EA | \$ 1,500.00 | \$ 24,000.00 | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 0 | EA | \$ 10,000.00 | \$ - | incl. footing |
| Gateway Markers | 2 | EA | \$ 50,000.00 | \$ 100,000.00 | incl. footing |
| Art Markers | 0 | EA | \$ 10,000.00 | \$ - | incl. footing |
| Subtotal | | | | | \$ 124,000.00 |
| Subtotal | | | | | \$ 492,217.20 |
| Survey (2%) | | | | | \$ 9,844.34 |
| Architectural / Engineering Fees (8%) | | | | | \$ 39,377.38 |
| Construction Management (10%) | | | | | \$ 49,221.72 |
| Contractor Overhead & Profit (10%) | | | | | \$ 49,221.72 |
| Contingency (20%) | | | | | \$ 98,443.44 |
| Total | | | | | \$ 738,325.80 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

WESTPORT ROAD - 40TH STREET

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|-------|------|---------------|---------------|-----------------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 12000 | SF | \$ 2.50 | \$ 30,000.00 | |
| Asphalt Street Removal | 2000 | SF | \$ 5.00 | \$ 10,000.00 | for crosswalk |
| Subtotal | | | | | \$ 92,000.00 |
| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
| PCC Pavement - 5" | 12000 | SF | \$ 5.00 | \$ 60,000.00 | SIDEWALK |
| New PCC Curb and Demo of Old | 160 | LF | \$ 25.00 | \$ 4,000.00 | |
| New Curb Ramp and Demo of Old | 8 | EA | \$ 1,500.00 | \$ 12,000.00 | |
| Integral Colored Concrete Crosswalk | 2000 | SF | \$ 45.00 | \$ 90,000.00 | 10" Thick |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 140 | LF | \$ 10.00 | \$ 1,400.00 | 24" W - 70 LF each |
| Subtotal | | | | | \$ 167,400.00 |
| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
| Street Trees | 21 | EA | \$ 500.00 | \$ 10,500.00 | |
| Amended Soil | 84 | CY | \$ 85.00 | \$ 7,140.00 | 4 CY per Tree |
| Mulch | 6.5 | CY | \$ 75.00 | \$ 487.50 | .3 CY per Tree |
| Perennials (In Tree Planters) | 204 | EA | \$ 15.00 | \$ 3,060.00 | 12 per planter |
| Drainage for Planters | 640 | LF | \$ 15.00 | \$ 9,600.00 | |
| Subtotal | | | | | \$ 30,787.50 |
| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
| Lighting and Electrical | 1 | LS | \$ 180,695.45 | \$ 180,695.45 | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 280,695.45 |
| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
| Edge Treatment | 150 | LF | \$ 300.00 | \$ 45,000.00 | private- not in total |
| Tree Grate | 0 | EA | \$ 3,000.00 | \$ - | incl. frame |
| Concrete Planter | 17 | EA | \$ 1,500.00 | \$ 25,500.00 | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 2 | EA | \$ 10,000.00 | \$ 20,000.00 | incl. footing |
| Gateway Markers | 0 | EA | \$ 50,000.00 | \$ - | incl. footing |
| Art Markers | 0 | EA | \$ 10,000.00 | \$ - | incl. footing |
| Mnookin Plaza Expansion | 1 | LS | \$ 100,000.00 | \$ 100,000.00 | |
| Subtotal | | | | | \$ 145,500.00 |
| Subtotal | | | | | \$ 716,382.95 |
| Survey (2%) | | | | | \$ 14,327.66 |
| Architectural / Engineering Fees (8%) | | | | | \$ 57,310.64 |
| Construction Management (10%) | | | | | \$ 71,638.30 |
| Contractor Overhead & Profit (10%) | | | | | \$ 71,638.30 |
| Contingency (20%) | | | | | \$ 143,276.59 |
| Total | | | | | \$ 1,074,574.43 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

40TH STREET - 41ST STREET

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|-------------------------------|------|------|--------------|--------------|---------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 3400 | SF | \$ 2.50 | \$ 8,500.00 | |
| Asphalt Street Removal | 0 | SF | \$ 5.00 | \$ - | for crosswalk |
| Subtotal | | | | | \$ 60,500.00 |

| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|------|------|-------------|--------------|--------------------|
| PCC Pavement - 5" | 3400 | SF | \$ 5.00 | \$ 17,000.00 | SIDEWALK |
| New PCC Curb and Demo of Old | 0 | LF | \$ 25.00 | \$ - | |
| New Curb Ramp and Demo of Old | 6 | EA | \$ 1,500.00 | \$ 9,000.00 | |
| Integral Colored Concrete Crosswalk | 0 | SF | \$ 45.00 | \$ - | 10" Thick |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 210 | LF | \$ 10.00 | \$ 2,100.00 | 24" W - 70 LF each |
| Subtotal | | | | | \$ 28,100.00 |

| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
|-------------------------------|-----|------|-----------|------------|----------------|
| Street Trees | 0 | EA | \$ 500.00 | \$ - | |
| Amended Soil | 0 | CY | \$ 85.00 | \$ - | 4 CY per Tree |
| Mulch | 0 | CY | \$ 75.00 | \$ - | .3 CY per Tree |
| Perennials (In Tree Planters) | 0 | EA | \$ 15.00 | \$ - | 12 per planter |
| Drainage for Planters | 0 | LF | \$ 15.00 | \$ - | |
| Subtotal | | | | | \$ - |

| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
|-----------------------------------|-----|------|--------------|--------------|---------------|
| Lighting and Electrical | 1 | LS | \$ 79,760.72 | \$ 79,760.72 | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 179,760.72 |

| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
|------------------------|-----|------|--------------|---------------|-----------------------|
| Edge Treatment | 350 | LF | \$ 300.00 | \$ 105,000.00 | private- not in total |
| Tree Grate | 0 | EA | \$ 3,000.00 | \$ - | incl. frame |
| Concrete Planter | 0 | EA | \$ 1,500.00 | \$ - | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 6 | EA | \$ 10,000.00 | \$ 60,000.00 | incl. footing |
| Gateway Markers | 0 | EA | \$ 50,000.00 | \$ - | incl. footing |
| Art Markers | 0 | EA | \$ 10,000.00 | \$ - | incl. footing |
| Subtotal | | | | | \$ 60,000.00 |

| | | | | | |
|---------------------------------------|--|--|--|--|----------------------|
| Subtotal | | | | | \$ 328,360.72 |
| Survey (2%) | | | | | \$ 6,567.21 |
| Architectural / Engineering Fees (8%) | | | | | \$ 26,268.86 |
| Construction Management (10%) | | | | | \$ 32,836.07 |
| Contractor Overhead & Profit (10%) | | | | | \$ 32,836.07 |
| Contingency (20%) | | | | | \$ 65,672.14 |
| Total | | | | | \$ 492,541.08 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

41ST STREET - 43RD STREET

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|-------|------|--------------|---------------|-----------------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 10500 | SF | \$ 2.50 | \$ 26,250.00 | |
| Asphalt Street Removal | 2000 | SF | \$ 5.00 | \$ 10,000.00 | for crosswalk |
| Subtotal | | | | | \$ 88,250.00 |
| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
| PCC Pavement - 5" | 10500 | SF | \$ 5.00 | \$ 52,500.00 | SIDEWALK |
| New PCC Curb and Demo of Old | 0 | LF | \$ 25.00 | \$ - | |
| New Curb Ramp and Demo of Old | 6 | EA | \$ 1,500.00 | \$ 9,000.00 | |
| Integral Colored Concrete Crosswalk | 2000 | SF | \$ 45.00 | \$ 90,000.00 | 10" Thick |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 70 | LF | \$ 10.00 | \$ 700.00 | 24" W - 70 LF each |
| Subtotal | | | | | \$ 152,200.00 |
| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
| Street Trees | 18 | EA | \$ 500.00 | \$ 9,000.00 | |
| Amended Soil | 72 | CY | \$ 85.00 | \$ 6,120.00 | 4 CY per Tree |
| Mulch | 5.4 | CY | \$ 75.00 | \$ 405.00 | .3 CY per Tree |
| Perennials (In Tree Planters) | 216 | EA | \$ 15.00 | \$ 3,240.00 | 12 per planter |
| Drainage for Planters | 700 | LF | \$ 15.00 | \$ 10,500.00 | |
| Subtotal | | | | | \$ 29,265.00 |
| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
| Lighting and Electrical | 1 | LS | \$ 91,406.90 | \$ 91,406.90 | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 191,406.90 |
| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
| Edge Treatment | 1060 | LF | \$ 300.00 | \$ 318,000.00 | private- not in total |
| Tree Grate | 0 | EA | \$ 3,000.00 | \$ - | incl. frame |
| Concrete Planter | 18 | EA | \$ 1,500.00 | \$ 27,000.00 | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 2 | EA | \$ 10,000.00 | \$ 20,000.00 | incl. footing |
| Gateway Markers | 1 | EA | \$ 50,000.00 | \$ 50,000.00 | incl. footing |
| Art Markers | 7 | EA | \$ 10,000.00 | \$ 70,000.00 | incl. footing |
| Revolving Art Piece | 1 | EA | \$ 25,000.00 | \$ 25,000.00 | incl. footing |
| Subtotal | | | | | \$ 192,000.00 |
| Subtotal | | | | | \$ 653,121.90 |
| Survey (2%) | | | | | \$ 13,062.44 |
| Architectural / Engineering Fees (8%) | | | | | \$ 52,249.75 |
| Construction Management (10%) | | | | | \$ 65,312.19 |
| Contractor Overhead & Profit (10%) | | | | | \$ 65,312.19 |
| Contingency (20%) | | | | | \$ 130,624.38 |
| Total | | | | | \$ 979,682.85 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX A - BLOCK BY BLOCK COST OPINIONS

43RD STREET - 44TH STREET

| Site Preparation | Qty | Unit | Unit Cost | Item Total | Remarks |
|-------------------------------|------|------|--------------|--------------|---------------|
| Mobilization | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Construction fencing | 1000 | LF | \$ 2.00 | \$ 2,000.00 | |
| PCC Sidewalk Pavement Removal | 5000 | SF | \$ 2.50 | \$ 12,500.00 | |
| Asphalt Street Removal | 0 | SF | \$ 5.00 | \$ - | for crosswalk |
| Subtotal | | | | | \$ 64,500.00 |

| Paving | Qty | Unit | Unit Cost | Item Total | Remarks |
|---------------------------------------|------|------|-------------|--------------|--------------------|
| PCC Pavement - 5" | 5000 | SF | \$ 5.00 | \$ 25,000.00 | SIDEWALK |
| PCC Curb | 0 | LF | \$ 25.00 | \$ - | |
| Curb Ramp | 4 | EA | \$ 1,500.00 | \$ 6,000.00 | |
| Integral Colored Concrete Crosswalk | 0 | SF | \$ 45.00 | \$ - | 10" Thick |
| Integral Colored Concrete Drive Apron | 0 | SF | \$ 20.00 | \$ - | 8" Thick |
| Secondary Crosswalk Stripe | 140 | LF | \$ 10.00 | \$ 1,400.00 | 24" W - 70 LF each |
| Subtotal | | | | | \$ 32,400.00 |

| Planting | Qty | Unit | Unit Cost | Item Total | Remarks |
|-------------------------------|-----|------|-----------|-------------|----------------|
| Street Trees | 9 | EA | \$ 500.00 | \$ 4,500.00 | |
| Amended Soil | 36 | CY | \$ 85.00 | \$ 3,060.00 | 4 CY per Tree |
| Mulch | 2.7 | CY | \$ 75.00 | \$ 202.50 | .3 CY per Tree |
| Perennials (In Tree Planters) | 108 | EA | \$ 15.00 | \$ 1,620.00 | 12 per planter |
| Drainage for Planters | 220 | LF | \$ 15.00 | \$ 3,300.00 | |
| Subtotal | | | | | \$ 12,682.50 |

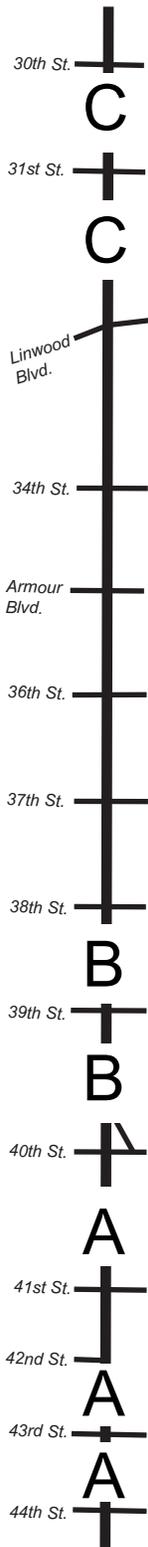
| Streetscape Utilites | Qty | Unit | Unit Cost | Item Total | Remarks |
|-----------------------------------|-----|------|--------------|--------------|---------------|
| Lighting and Electrical | 1 | LS | \$ 20,891.50 | \$ 20,891.50 | |
| Curb Inlet | 5 | EA | \$ 4,000.00 | \$ 20,000.00 | |
| Storm Pipe Relocation | 100 | LF | \$ 100.00 | \$ 10,000.00 | |
| Fire Hydrant Relocation | 2 | EA | \$ 5,000.00 | \$ 10,000.00 | |
| Miscellaneous Utility Relocations | 1 | LS | \$ 50,000.00 | \$ 50,000.00 | |
| Pipe Reaming and Cleaning | 1 | LS | \$ 10,000.00 | \$ 10,000.00 | |
| Subtotal | | | | | \$ 120,891.50 |

| Streetscape Structures | Qty | Unit | Unit Cost | Item Total | Remarks |
|------------------------|-----|------|--------------|--------------|-----------------------|
| Edge Treatment | 150 | LF | \$ 300.00 | \$ 45,000.00 | private- not in total |
| Tree Grate | 0 | EA | \$ 3,000.00 | \$ - | incl. frame |
| Concrete Planter | 9 | EA | \$ 1,500.00 | \$ 13,500.00 | 6"THK. 4' x 8' x 10"D |
| Neighborhood Markers | 2 | EA | \$ 10,000.00 | \$ 20,000.00 | incl. footing |
| Gateway Markers | 1 | EA | \$ 50,000.00 | \$ 50,000.00 | incl. footing |
| Art Markers | 6 | EA | \$ 10,000.00 | \$ 60,000.00 | incl. footing |
| Subtotal | | | | | \$ 143,500.00 |

| | | | | | |
|---------------------------------------|--|--|--|--|---------------|
| Subtotal | | | | | \$ 373,974.00 |
| Survey (2%) | | | | | \$ 7,479.48 |
| Architectural / Engineering Fees (8%) | | | | | \$ 29,917.92 |
| Construction Management (10%) | | | | | \$ 37,397.40 |
| Contractor Overhead & Profit (10%) | | | | | \$ 37,397.40 |
| Contingency (20%) | | | | | \$ 74,794.80 |
| Total | | | | | \$ 560,961.00 |

*note: see pages 42-43 for inclusions and exclusions to this cost opinion

APPENDIX B - STREET TREE KEY



The following is an outline of the trees to be used with this Master Plan and the locations for each.

| Key | Common Name | Scientific Name |
|-----|-------------------------|--|
| A. | Columnar Norway Maple | <i>Acer platanoides</i> "Columnaris". |
| B. | Princeton Sentry Ginkgo | <i>Ginkgo biloba</i> 'Princeton Sentry' (Male Only) |
| C. | Skyline Honeylocust | <i>Gleditsia triacanthos</i> var. <i>inermis</i> 'Skyline' |

Notes:

- All trees shall be nursery grown, balled and burlapped and shall be a minimum of 3.5" caliper and limbed up to a minimum of 6' above the sidewalk.
- All trees of the same species shall be matching, vigorous, healthy, true to form and symmetrical in nature.
- All trees shall be true to species and variety and conform to the sizes specified.

APPENDIX C - FUNDING SOURCES

The following is a list of the possible funding sources explained:

Tax Increment Financing Funds (TIF)

Tax Increment Financing (TIF) is a financing and development tool that allows future real property taxes and other taxes generated by new development to pay for costs of construction of public infrastructure and other improvements. (As quoted from the Economic Development Corporation of Kansas City, Missouri website)

Community Improvement District Funds (CID)

According to Missouri State Statute, Community Improvement Districts (CIDs) allow property owners to tax themselves in order to supplement city-provided services such as policing and maintaining public areas. As an investment tool, they not only enhance city services and public investment, but leverage greater private resources as well. (As quoted on the MainCor website)

Public Improvemnts Advisory Committee Funds (PIAC)

The Public Improvements Advisory Committee consists of 13 people, two from each council district and a chairperson, appointed by the mayor and City Council. The group was created in 1983 by City Council Resolution 55417 and reaffirmed by City Council Resolution 62031. The PIAC's primary function is to solicit citizen input and make recommendations regarding both the citywide and neighborhood portions of the capital budget. The PIAC holds a series of public hearings beginning in early summer. These hearings provide citizens with an opportunity to express their opinions, concerns and project requests regarding the forthcoming capital budget. This committee finishes its year by submitting its balanced five-year capital improvements program and neighborhood recommendations in late November. (As quoted on the Kansas City, Missouri website)

Surface Transportation Program - Transportation Enhancement Funds (STP-TE)

Transportation Enhancement (TE) activities offer funding opportunities to help expand transportation choices and enhance the transportation experience through 12 eligible TE activities, related to surface transportation, including pedestrian and bicycle infrastructure and safety programs, scenic and historic highway programs, landscaping and scenic beautification, historic preservation, and environmental mitigation. TE investments benefit communities through rehabilitation of historic facilities related to transportation, renovated streetscapes, rail-trails and other transportation trails, transportation museums, and scenic and historic highway program visitor centers. (As quoted from the Federal Highway Administration website)

APPENDIX C - FUNDING SOURCES

Federal Highway Administration Congestion Mitigation and Air Quality Improvement Program (CMAQ)

In 1990, Congress amended the Clean Air Act to accelerate America's efforts to attain the NAAQS. The amendments required further reductions in the amount of permissible tailpipe emissions, initiated more stringent control measures in areas that still failed to attain the NAAQS (nonattainment areas), and provided for a stronger, more rigorous linkage between transportation and air quality planning. The following year, Congress adopted the Intermodal Surface Transportation Efficiency Act (ISTEA) of 1991. This law authorized the Congestion Mitigation and Air Quality Improvement Program (CMAQ) to provide funding for surface transportation and other related projects that contribute to air quality improvements and congestion mitigation. The CAA amendments, ISTEA and the CMAQ program together were intended to realign the focus of transportation planning toward a more inclusive, environmentally-sensitive, and multimodal approach to addressing transportation problems. (As quoted from the Federal Highway Administration website)